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HISTORIC FAIRFIELD DISTRICT
OF DOWNTOWN LAKEVILLE

DESIGN
GUIDELINES



HISTORIC FAIRFIELD DISTRICT
OF DOWNTOWN LAKEVILLE

DESIGN
GUIDELINES

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HISTORIC FAIRFIELD DISTRICT
OF DOWNTOWN LAKEVILLE

**DESIGN
GUIDELINES**

-SECTION ONE-
PROJECT ONE

1.0 PROJECT SUMMARY

1.1 INTRODUCTION

The purpose for these Design Guidelines are to reflect and enhance the historical character of the original downtown district which will create a pedestrian oriented place where the citizens of Lakeville can show their pride, experience their community's history, and improve their quality of life. These Design Guidelines shall serve to set expectations for the City of Lakeville who will be assisting prospective developers with the planning of future projects.

Since these Design Guidelines will be used to evaluate how development should occur within the Fairfield District, the vision and image as developed in the Corridor and Gateway Design Study dated August 2, 1999 shall be incorporated into the Design Guidelines.

1.2 VISION AND IMAGE

During the Ideation Workshop held on February 23, 1999, the participants of this workshop developed a series of "Desired Image[s] and Vision[s]." Some of these images and visions have been reproduced as they relate to the historic character of Fairfield District.

- "Preserve green spaces and open spaces that provide year round beauty."
- "Preserve and enhance historic architecture."
- "Preserve and enhance rural image/charm to maintain that small town feeling."
- "Redevelop commercial and residential historic buildings."

Based upon these observations, the design vision for the Fairfield District of Down-

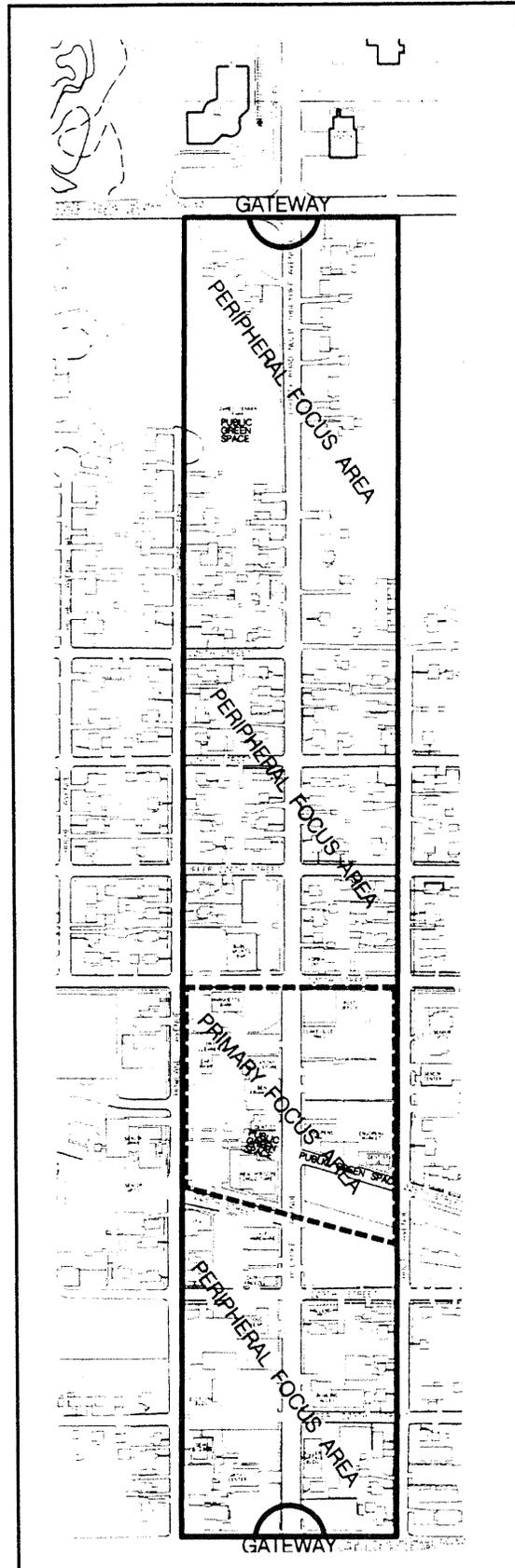


Figure 1.3a
Primary and Peripheral Focus Areas

town Lakeville shall be to enhance and re-establish this area as a vital urban commerce center while maintaining the historic character and small town feel of a rural community.

1.3 STUDY AREA AND GATEWAYS

This study encompasses a Primary Focus Area and Focus Area (Figure 1.3a, page 2). The Primary Focus Area is located on Holyoke Avenue from 207th Street to 208th Street (north of Ace Hardware) and extends one block west to Howland Avenue and one block east to Holt Avenue.

The Peripheral Focus Area includes Holyoke Avenue from Upper 206th Street to Lakeville Boulevard (Highway 50) and extends one block west to Howland Avenue and one block east to Holt Avenue. Gateway intersections as determined in the Corridor and Gateway Design Study are defined as the North Gateway (at Highway 50 and Holyoke) and should be set aside for future planning of ornamental architectural entrances to the Fairfield District as determined and developed in the study. (See Figure 1.3b)

1.4 MAIN STREET

Holyoke Avenue was originally called Main Street. We recommend changing

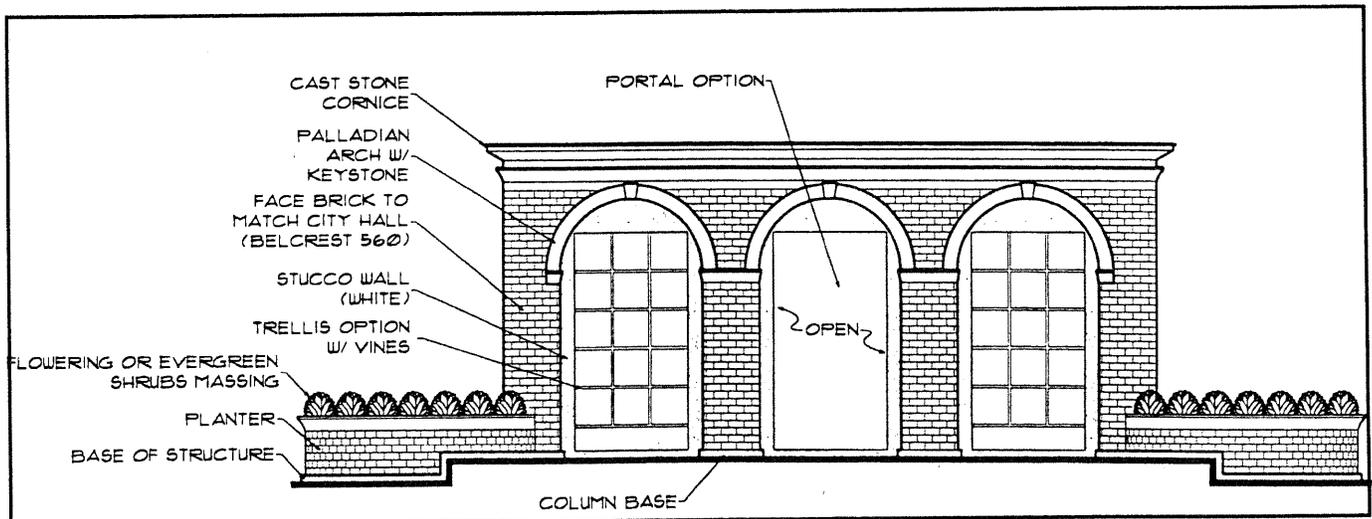
the name of Holyoke Avenue through the Fairfield District as Main Street. This area encompasses the historic core of the Downtown and consequently, where the principal historic structures are located. Based upon the vision and image previously described, redevelopment and new development should be focused on destination uses within this Primary Focus Area such as a Post Office, movie theater, restaurant, specialty shops, professional offices, service, retail shops, etc.

For the purpose of this report we are referring to Holyoke Avenue as Main Street.

1.5 HISTORICAL SIGNIFICANCE

Founded in 1858 by a small group of settlers, Lakeville Township was established at what is now called the Dodd and Highway 50 site. In 1878, the village had a larger base population and organized the incorporation of the township. A fire destroyed the original downtown and subsequently the town of Lakeville was then moved to the current location of Holyoke Avenue between 207th and upper 209th. This area was formerly known as Fairfield, which provides the basis for changing the name of the downtown to "Historic Fairfield District of Downtown Lakeville."

*Figure 1.3b
Fairfield District Gateway as per details in Corridor & Gateway Design Study by Barton-Aschman Associates, Inc.*



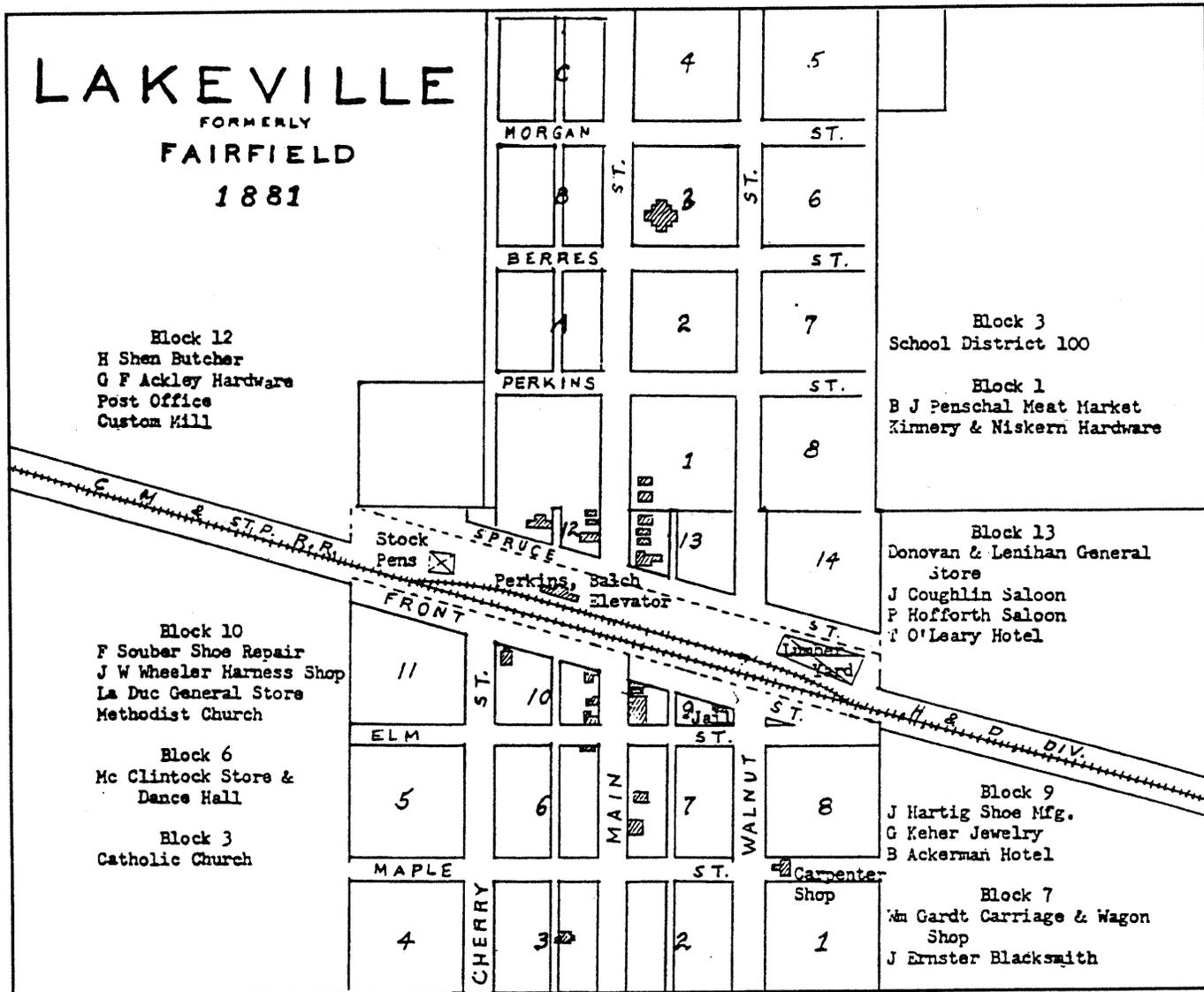


Figure 1.5a
Map of downtown Lakeville circa
1881.

Because of the immigrant population explosion in the latter half of the 19th century, a downtown sprang up overnight. A review of an 1881 map (see Figure 1.5a) of the original downtown shows that the downtown consisted of typical agrarian support companies such as a harness shop, two general stores, two butcher shops, two hardware stores, a Post Office, custom mill, blacksmith, carriage and wagon shop, two saloons and two hotels. The 1881 downtown was constructed in close proximity to the main railway supply lines which traveled diagonally crossing Main Street. This general area is where the present day historic downtown remains and is

the Primary Focus Area of this study.

The buildings of the 1881 downtown were constructed quickly to facilitate the needs of the population boom with materials that were inexpensive, easily attainable, and quick to construct, such as wood frame structures and wood siding. Following a fatal fire in one of the general stores on Main Street in 1895 (now Holyoke Avenue), the City Government passed an ordinance that all buildings facing Main Street shall be constructed of brick or stone and be "impervious to fire." Thus, the historic buildings which stand today are direct descendants of that ordinance.

Much of the original historic architectural style still remains in the downtown. Wide sidewalks connect the heavily traveled Holyoke Avenue to the buildings. A City Park was established on a portion of the previous railway. Two story brick structures, which accommodate retail uses on the first level and offices on the second level, are hidden behind layers of paint and earlier decades of “modernization.” See Figure 1.5c for a view of the brick structures along Main Street during the 1940’s.

The original grid street pattern still remains intact with some minor alterations. The 1881 Main Street is now called Holyoke Avenue. Front and Spruce Streets which traveled adjacent to and on either side of the railway were renamed 208th Streets. The other original 1881 street names were subsequently changed to corresponding numerical street names. See Figure 1.5b for a view of the grid street pattern

The remaining original historic buildings on Main Street include the Bank Building (now Marlene’s Tailoring), the Fire Hall (now Lakeville Publishing), single story retail (now A+ Driving School and Clay Cottage Ceramics), and an original tavern (now Lakeville Trophy).

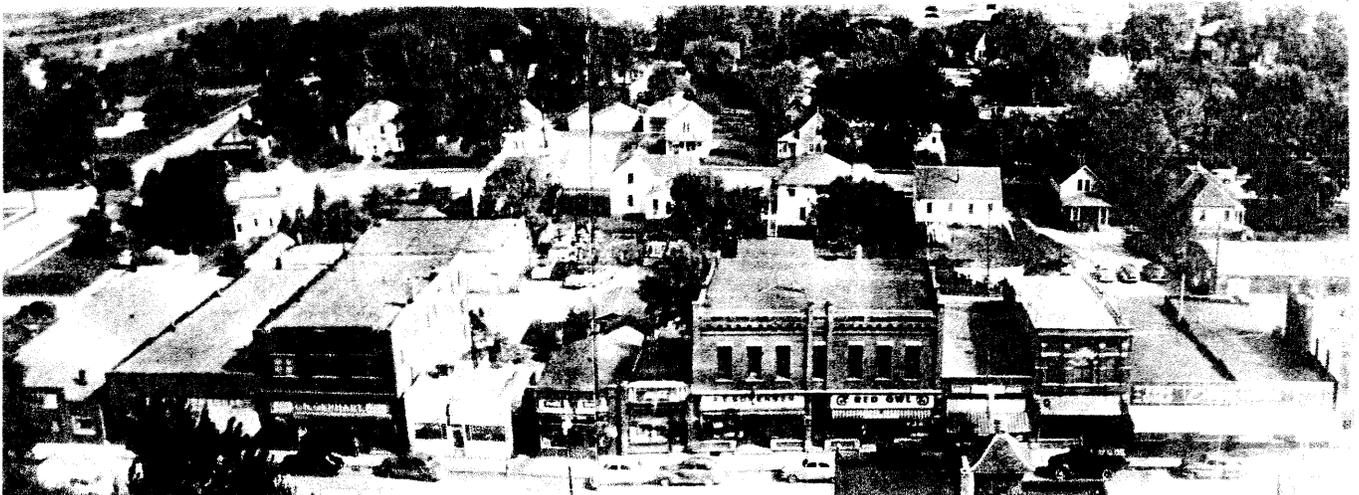


Figure 1.5b
View of bank building on 208th Street and Holyoke Avenue circa 1940’s.

1.6 CASE STUDIES

In order to provide a basis of comparison, a part of this study involves evaluating the Fairfield District in comparison to other successful small downtown areas which have been recently renovated or have been under continuous improvements for many years.

Figure 1.5c
Original two story buildings which house retail on the first level and office on the second level. Circa 1940’s.



1.6.1 50th and France– Edina.

This quaint area of retail shops, restaurants, and professional offices is located 20 blocks north of Highway 62 and Southdale Mall. It has enjoyed years of success as a revitalized urban shopping district. This area is successful for the following reasons:

- Compact high density buildings which feed off each other and address the adjoining streets. See Figure 1.6.1a.
- Free of charge shared parking ramps located behind the north and south blocks provide ample parking for most activities.
- Organization of special annual events such as the Art Street Fair provide ongoing exposure to the area and potential customers.
- Mixed destination uses such as retail shops, offices and service business keep the area vital. Restaurants, the movie theater and grocery store provide stable off-peak consumers.
- Self-imposed restrictions on the quality of building materials, design and density attract high-end tenants. A variety of architectural styles have evolved over time and are encouraged for new developments.
- Heavily landscaped streetscapes create a pedestrian oriented environment that offers a variety of activities in a user friendly environment for window shopping and pedestrian circulation.
- Traffic is restricted to two lanes with parallel on-street parking and paver crosswalks which identifies areas for pedestrian circulation and serves to slow traffic and allow for a more pedestrian friendly environment.
- All sidewalks are constructed of brick pavers.
- All new buildings are constructed using sand-molded modular sized brick to reflect a historic character and incorporate a more human scale to the buildings.

*Figure 1.6.1a
50th & France –
The Edina theater provides an entertainment activity as well as a signature architectural element for the Mainstreet.*



*Figure 1.6.1b
50th & France –
Small shops with awnings and wide sidewalks provide a good pedestrian environment.*



- All buildings are arranged to address the adjacent street by being set as close to the street as possible to create a high density urban environment.
- Awnings provide a visual break in the streetscape and add life and color to the structures. See Figure 1.6.1b.
- Customer and employee parking are provided in structured parking ramps behind the Main Street buildings with access alleys to the Main Street sidewalk.

1.6.2 Downtown Hopkins.

This historic downtown has experienced substantial renovation for a portion of the Main Street of Hopkins which incorporates a large community center with multiple streetscape improvements. The entire historic downtown has undergone many different design renovations over the years. This area is successful for the following reasons:

- Historic buildings like the Opera Hall were retained and converted to new uses.
- New buildings are designed to fit into the density and architectural character of the historic downtown. Materials and details with a similar color and texture fit in nicely with existing historic buildings.
- Awnings provide user signage, promote window shopping by providing a covered walkway and bring two-story buildings down to a more human scale.
- High density of buildings provide a feeling of enclosure, much in the same manner as an indoor mall does.
- A new cinema adds an entertainment aspect to the downtown. See Figure 1.6.2a.
- The Hopkins Center for the Arts is built directly on Main Street to emphasize the civic character of Hopkins. See Figure 1.6.2b.

*Figure 1.6.1c
50th & France –
Restrictions on the design of buildings as well as the quality of materials attracts high end retailers.*



*Figure 1.6.2a
Downtown Hopkins –
The design of the new cinema creates a Main Street feel.*



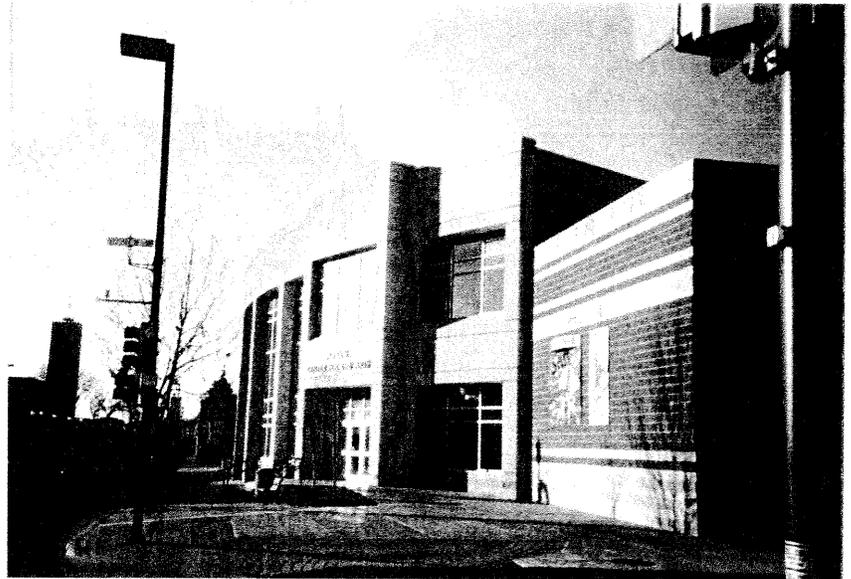
- Limited parking is available on the street and on adjacent side streets. Parking lots are not visible from Main Street and are located in secondary locations but shared by many businesses.
- Streetscape improvements such as pavers, benches, and ornamental light poles with banners add a common design theme to the Main Street.
- Curbed islands create traffic calming which adds to the pedestrian comfort and safety.
- An ornamental clock tower serves as an identifiable downtown landmark element visible on Main Street.
- Restaurants, cafes and coffee shops provide activity, destination uses for visitors as well as citizens, and vital services for people who work in the downtown every day.

1.6.3 Lake Street-Downtown Wayzata.

This area incorporates historically restored low-rise buildings with a mix of high density multi-family and other new buildings to achieve a successful balance of retail, office, restaurants, services, offices, cafes, and housing. See Figure 1.6.3. The following lists these successes:

- Incorporates small, unique specialty shops throughout the downtown district.
- The Caribou Coffee shop attracts many local residents as a relaxing and meeting place.
- Small seating areas have been provided by the city and the coffee shop for visitors to gather.
- Single level, zero lot line shops create synergy with each other.
- Unique restaurants attract evening customers.
- Although there is no theater on main street, a cinema marquee adds a sense of history as well as provides a sign to promote the shops inside the building.

*Figure 1.6.2b
Downtown Hopkins –
The Hopkins Center for the Arts is built right on Main Street to emphasize the civic character of Hopkins.*



*Figure 1.6.3
Downtown Wayzata –
Incorporates small unique specialty shops throughout the downtown.*



- A renovated train depot is used as the Chamber of Commerce building and a senior community center. The area provides a landscaped link between downtown and the depot for pedestrian traffic.
- On street parking and wide sidewalks create a user friendly, pedestrian oriented environment.
- Centralized municipal parking lots provide strategically located parking spaces for customers and employees for easy traffic flow.

HISTORIC FAIRFIELD DISTRICT
OF DOWNTOWN LAKEVILLE

**DESIGN
GUIDELINES**

-SECTION TWO-
HISTORIC DISTRICT ANALYSIS

2.0 HISTORIC DISTRICT ANALYSIS

2.1 HISTORICAL PATTERNS

The long term survival of the Fairfield District is dependent upon establishing destination uses and providing interesting and exciting outdoor areas for seasonal festivals and activities to give residents and visitors a reason to come to the Fairfield District.

- Development should occur within the existing street grid of the downtown.
- Development should utilize existing edges to create boundaries between existing and proposed land uses to reflect the history of this district. For example, the 1881 town map (See Figure 1.5a, page 4) shows Spruce Street to the north and Front Street to the south of the railroad tracks that diagonally bisected the original downtown. These streets, or portions thereof, could be reclaimed and redeveloped, reflecting the historical significance of the railroad to the town.
- Future development should reflect the history of the district with an architectural form, site arrangement of the buildings addressing Main Street and public spaces being created to reinforce the built environment.

2.2 MIXED USES

Mixed uses promote a variety of development uses for the Fairfield District which will attract residents and visitors during all times of the day.

- Encourage a multitude of uses and activities including retail, commercial, office, public/civic, multifamily housing and recreational/entertainment

uses. These uses may include a retail post office store, movie theater, convenience/specialty grocery store, drugstore, specialty shops, restaurants, residential apartments, arts center, daycare, professional offices, expanded senior center, farmers market, visitors center/Chamber of Commerce building, history center, bed and breakfast hotels, religious structures, etc.

- Locate uses where parking can be shared by day and night time users.

2.3 PEDESTRIAN FRIENDLY

By keeping the Fairfield District within the original confines of the Primary Focus Area (see Figure 1.3a, page 2), a compact downtown will result in a pedestrian friendly, small town atmosphere.

- New buildings should be located with zero setbacks. This will provide a strong edge to Main Street with entrances facing Main Street. This will encourage pedestrian activity and produce buildings that are sympathetic to the street.
- Retail shops should be located on the first level with glass storefronts to encourage window shopping, while offices and/or apartment buildings should be located on the second or third levels. Two and three story buildings should be encouraged.
- Sidewalks which link all the shops on Main Street should be landscaped with trees, vegetation, planters, ornamental railings, benches for resting and viewing the Fairfield District, different paving materials, bike racks, etc. to promote non-vehicular traffic.
- Locate areas for sitting and viewing

the Fairfield District that provide a sense of security and peacefulness to the users.

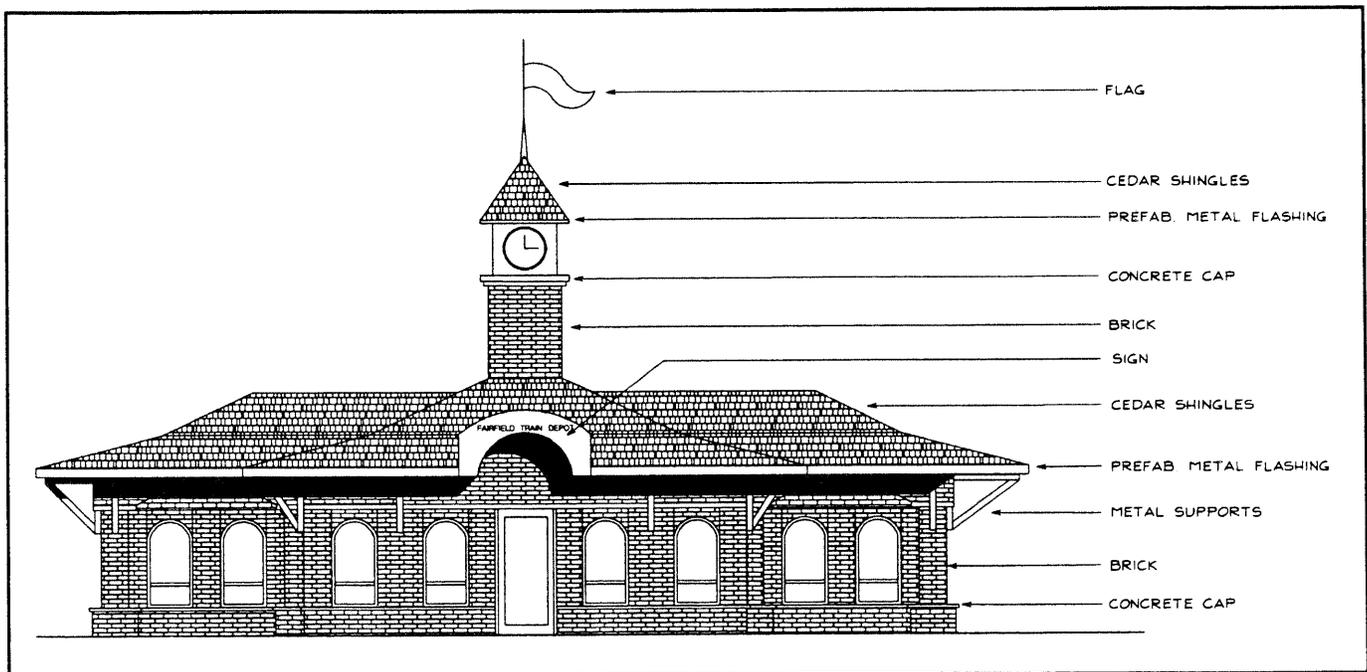
- Provide continuous walking paths which connect all uses of the Fairfield District and form a strong connection to the surrounding neighborhoods and the city’s trail system. All walking paths should converge at the City Park, otherwise known as the Town Square.
- Locate new buildings in clusters to allow easy pedestrian movement between shops and uses.
- When existing buildings and storefronts are renovated allow for new destination uses for the long-term success of the district. Existing non-retail type uses could be relocated to allow for expansion of new uses at the street level to the second levels of the prospective building.

2.4 PARKING AND VEHICULAR TRAFFIC

The Fairfield District should be as comfortable for pedestrians as it is for vehicular traffic, recognizing that drivers become pedestrians as soon as their vehicle is parked.

- Parking areas should be located behind buildings and consolidated in areas where several businesses can be served. Shared parking should be encouraged to reduce the total number of parking spaces needed within the Fairfield District.
- Truck Loading zones should be located at the rear of the buildings not visible to Main Street and screened from parking areas.
- On-street parallel parking should be encouraged on Main Street and within the Fairfield District in general. Islands should be incorporated to break up the length of parallel parking and to provide landscaped areas or plazas located in front of significant buildings.
- Pedestrian traffic should be character-

*Figure 2.5.2a
A historically referenced Train Depot could be used for weekly Farmers Markets.*



istic of the district. Pavers should be incorporated into the street material at all pedestrian crosswalks. Crosswalks should have a structure highly visible from the vehicle and pedestrian traffic denoting the entrance onto the street.

2.5 TOWN SQUARE

The existing City Park is located directly adjacent to the corner with the most historic significance in the district. Essentially, this is the center of the original downtown and the location where the railroad intersected the downtown. As thus, is the logical location to re-establish the Town Square. The area could serve as the focal point for all outdoor activities within the Fairfield District during the seasonal months of the year as well as possibilities for winter activities such as carnivals, festivals, contest etc. (See Figure 2.5, page 15)

2.5.1 Activities.

Activities are another means of bringing people into the district. The Fairfield District should be the core of any future city sponsored activities and events.

Activities could include:

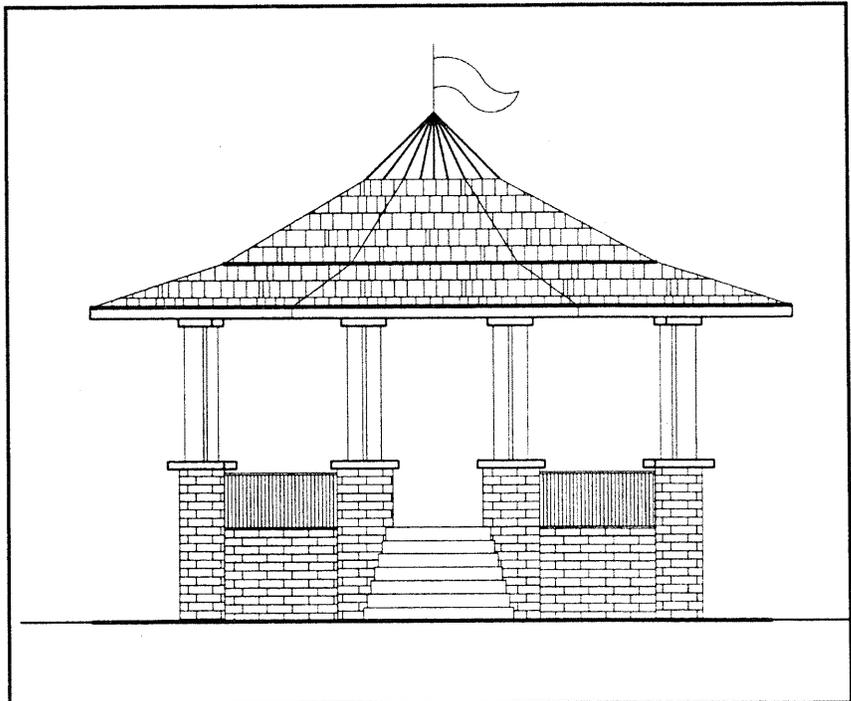
- Central hub for activity information/ visitor’s center/Chamber of Commerce office.
- Site of a future Fall Festival and/or Winter Carnival.
- Summer festivals such as a weekly Farmer’s Market. Future site for annual Street Dance.
- Various Pan-O-Prog festivities.
- Sidewalk sales or sidewalk art fairs.

2.5.2 Design Features

Design Features could include:

- A new historically referenced Train Depot and Train Shed which could be used for a weekly Farmer’s Market. (See Figure 2.5.2a, page 12)
- Information Kiosk.
- Gazebo for gatherings and music festivals. See Figure 2.5.2b.
- Streetscape features including trees and variety of seasonal plant material, benches, lighting, water fountain, etc.
- Allow the design of the Town Square to spill out into Main Street and 208th Street. Make this intersection the most important design element in the district by extending the paving patterns into the street which would enhance the design of the existing park but allow for larger uses. For example, an annual Street Dance could attract crowds of people and close off and redirect traffic for the evening. This would also signify this area as the

*Figure 2.5.2b
A historically referenced gazebo for gatherings and music festivals.*



center of Downtown.

2.6 HISTORICAL DOWNTOWN IDENTITY

Consideration should be given to renaming the downtown district as Fairfield Center “Lakeville’s Historic Downtown” to reflect back and borrow from Lakeville’s historic past. Additionally, Holyoke Avenue from 206th Street (at the south) to Highway 50 (at the north) should be renamed Main Street.

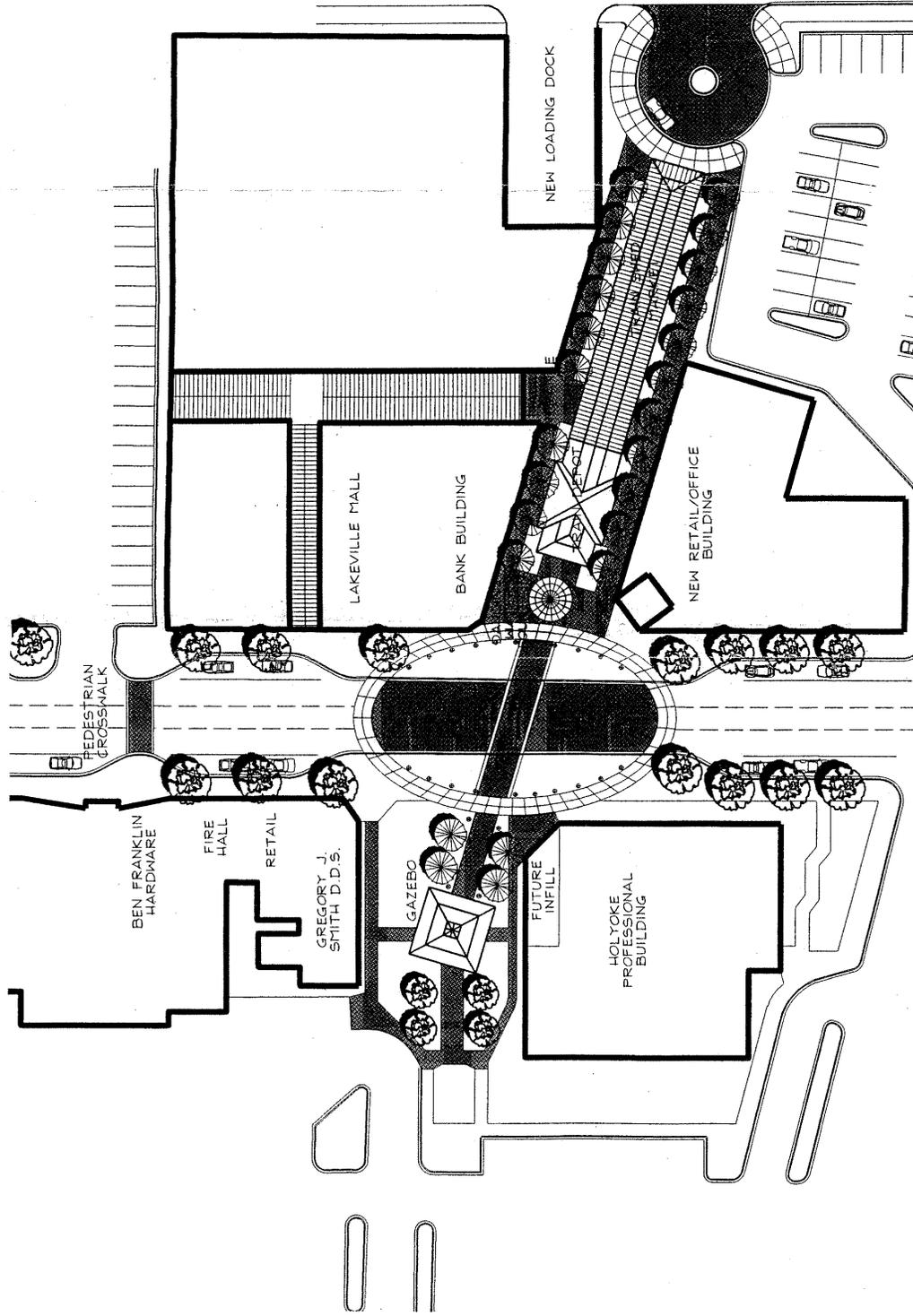


Figure 2.5
Proposed Town Square

3.0 COMMERCIAL SITE DEVELOPMENT DESIGN GUIDELINES

3.1 BUILDING SETBACKS

The Fairfield District should have consistent setbacks to establish a logical progression of densities as one approaches the Primary Focus Area. Maintaining these setbacks reinforces a specific design pattern along with establishing a lot coverage ratio will ensure that the available sites will be developed to their full potential.

The goal in the Primary Focus Area should be to locate any new construction on Main Street with a zero front yard setback and minimize any gaps between the buildings. Building entrances and storefronts should face Main Street. A continuous line of buildings located up to the sidewalk while not leaving any gaps helps to unify the street (see Figure 3.1.1).

Outside the Primary Focus Area the front yard setbacks should be pushed back to 10 feet to create a transition between the downtown area and the lower density areas. Side yard setbacks should be a minimum of 5 feet on one side and 15 feet on the other side, with no buildings closer than 20 feet. By pushing the buildings to one side of the lot it creates a more usable area side yard instead of two unusable side yards.

A setback is defined as a minimum distance from a right-of-way in which no structure may encroach. See Figure 3.1.2, (page 18) for recommended setbacks.

3.2 LOT COVERAGE

The Historic District shall have consistent Lot Coverage Ratios in order to provide a consistent historical design. The Lot Coverage Ratio is used to determine the over-

all site coverage density. This is based upon the area (in square feet) of the site in comparison to the area (in square feet) of the first floor of the building. See Figure 3.2, (page 19) for Lot Coverage Ratios.

3.3 PARKING

3.3.1 Pedestrian Alleys.

The buildings should be the main focus of the design. The parking lots should be secondary to the buildings. Parking lots

*Figure 3.1.1
A continuous line of buildings located up to the sidewalk helps to unify the street.*



should be located with easy access to the places of business. Parking lots should be located behind buildings and buildings should have occasional gaps where pedestrians can access the parking lots through the use of alleys. Alleys shall be a minimum of 10'-0" to 20'-0" wide and shall have brick pavers as a finished walking surface. Whenever possible, a portion of the shop's storefront should open to the alley. Storefront windows shall be accessible to the alley to create a window shopping experience. See photographs from 50th and France Figure 3.3.1a (page 20). See Figure 3.3.1b, (page 20) for Pedestrian Alley Access planning.

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-SECTION THREE-
COMMERCIAL SITE DEVELOPMENT
DESIGN GUIDELINES

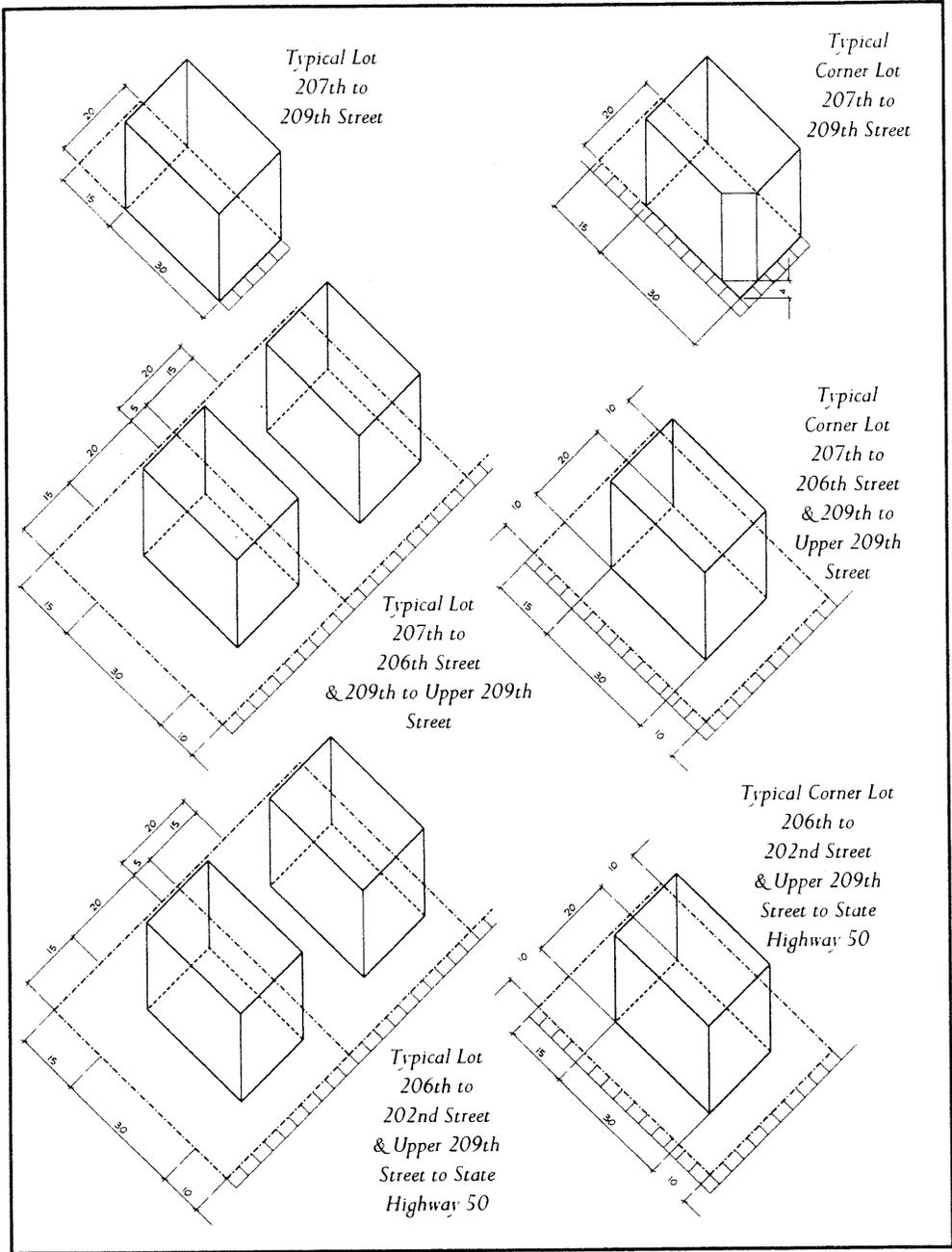


Figure 3.1.2 – Recommended Setbacks

3.3.2 Parking Buffers.

Parking areas shall be separated from buildings and public sidewalks with a landscape buffer. Earth berming in combination with ornamental iron fencing and plant material shall be used whenever possible to minimize the visual impact of large expanses of parking lots from public view. Berms shall not be continuous but shall

have breaks with fencing used in the breaks to connect the berms. The maximum length of a berm shall be 100'-0" and the minimum length shall be 20'-0". The maximum height of the highest portion of any berm shall be 48" and the minimum height of the highest portion of the berm shall be 18". The maximum slope on any berm shall be 30% grade.

Landscape buffers shall consist of landscape materials and vegetation and/or ornamental iron fences. Ornamental iron fences shall be at least 30% opaque and 70% open in all seasons. Height of such fence shall be 30" to 38". Ornamental iron fences shall be made of powder coated steel with welded joints. Color shall be black with a forest green railing. See Figure 3.3.2, (page 21) for ornamental fences.

3.3.3 Parking Stall Dimensions and Requirements.

Parking requirements shall be 3 stalls per 1000 SF of retail space (gross area) and office space (gross area) and 1 space per 80 square feet of total restaurant space. On street parking spaces which front the particular building shall be included within the parking lot stall requirements. All on-street parking shall be parallel parking. All parking lot stalls shall be 90 degree angle parking. Back-to-back parking stalls shall be 9'-0" wide by 20'-0" deep. Parking stalls which overhang a sidewalk or green space shall be 9'-0" wide by 18'-0" deep. Sidewalks with cars that overhang shall be a minimum of 6'-0" wide. Drive aisles shall be 24'-0" wide. See Figure 3.3.3a (page 21).

All curbs for parking lots, aprons, etc. shall be B612 design as shown on Figure 3.3.3b (page 21).

3.3.4 Preferential Parking.

Create employee parking in remote areas to allow for customer parking closer to the building entrances. Prohibit employees from parking on the street. All street parking should be designated and enforced as 90 minute maximum parking only.

3.4 LIGHTING

The existing street lighting has historic design elements and should be enhanced by adding more lights to the sidewalks and creating a strong repetition or pattern

throughout the Fairfield District. Any new street lighting shall match the existing fixtures. The proposed fixtures shall be spaced at 50'-0" minimum and 75'-0" maximum along Main Street.

The fixture pole shall be Lumec #Washington Style R54. All poles shall have 20A or 50A (catalog #R54-14.5-LMSP9333C) outdoor rated receptacle installed at 14'-6" above finished grade. All poles shall have banner arms and flag holder catalog #R54-14.5-LMS933B. Poles shall be Bronze color. The Acorn style globes (catalog # K118E-2-NSP-1) shall be 150W High Pressure Sodium Lamps with high performance lenses. All fixtures shall have a photocell and a HID-NPF ballast and socket assembly (supplier is King Luminaire catalog # K118EAR215HPS120K16NSPBZ). See Figure 3.4b (page 22)..

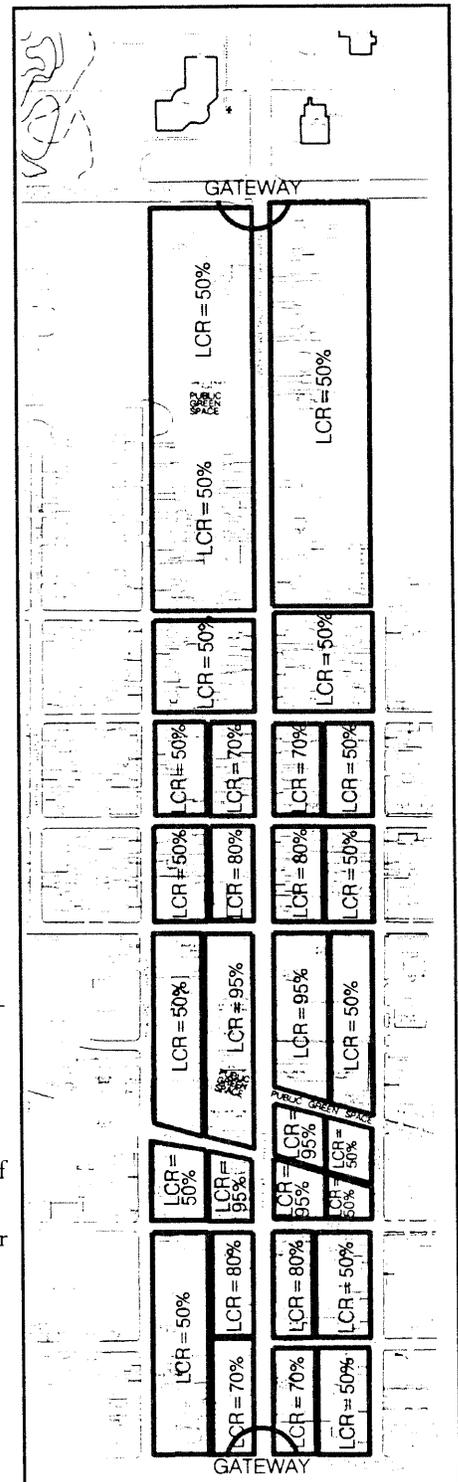
3.5 LANDSCAPING

3.5.1 Street Trees.

All trees located on Main Street shall be of deciduous species. The minimum caliper size allowed to be planted shall be 4" and shall have a decorative steel fence installed around the tree for protection (see Figure 3.5.1, page 22). The decorative fence shall have a powder coated finish and shall be at least 42" above finished grade in height.

Trees must be placed a minimum of 30'-0" and a maximum of 45'-0" apart. This will allow for the proper maturation of the tree. Trees should not conflict with the street lighting. Street trees on 208th Street shall be conifer trees, 16' in height when planted and trimmed up for pedestrian traffic.

Figure 3.2 – Lot Coverage Ratios



3.5.2 Landscape Buffer Trees.

Landscape buffer trees may be a combination of deciduous and conifer varieties. If deciduous trees are used, at least 50% of the deciduous trees must be ornamental flowering trees. The remainder of the deciduous trees may be a non-flowering over story species. Any conifers installed must be at least 10'-0" in height. Specific plant material varieties are identified in the Corridor & Gateway Design Study.

3.5.3 Miscellaneous Landscape Elements.

Removable planters, trash receptacles, benches, etc. shall be of steel construction and have a powder coated finish. Examples of these items are shown on Figure 3.6.2 (page 22), and shall be placed as per the Proposed Site and Landscape Plan.



Figure 3.3.1a
Alley link example between Main Street and parking lots.

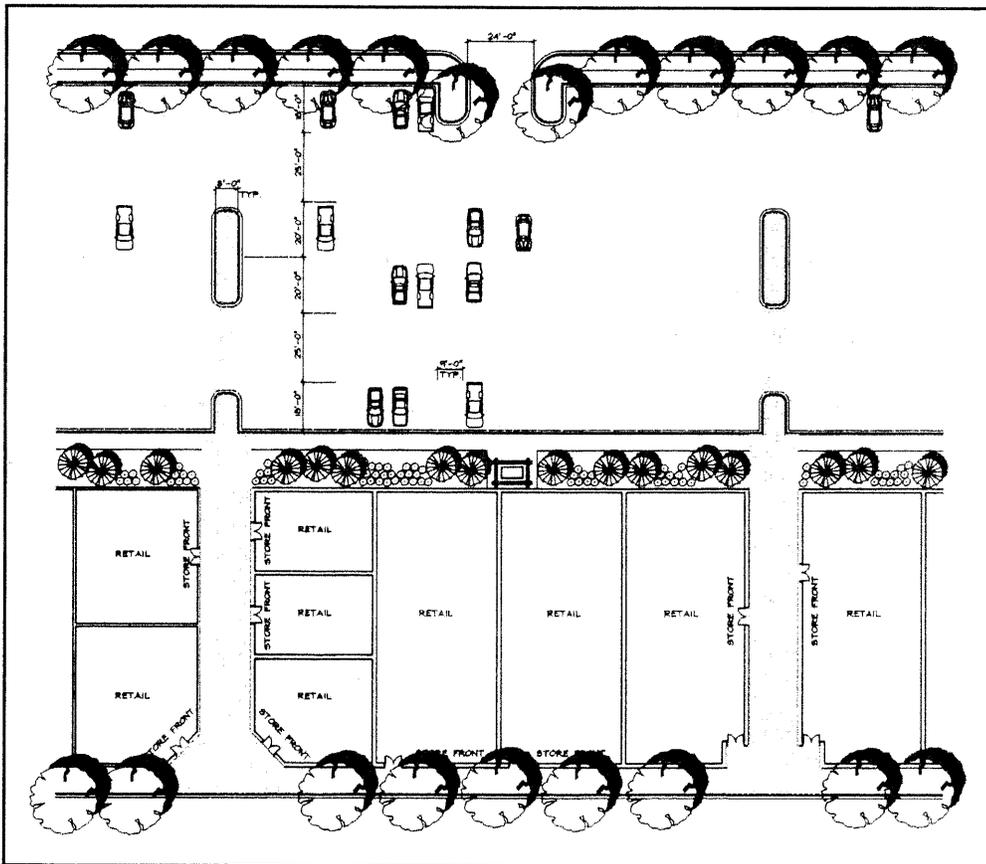


Figure 3.3.1b
Pedestrian Alley Access planning plan.

3.6 SIDEWALKS

3.6.1 Pavers.

All new sidewalks located on Main Street in the Historic District shall utilize pavers with a B612 concrete curb. The pavers shall have a regular paving pattern with special paving features at intersections of vehicular traffic and pedestrian traffic. For example, the intersection where the Pedestrian Alleys meet the typical Main Street sidewalk should be differentiated with a specifically designed paving pattern. In addition, crosswalks through the asphalt streets should integrate pavers into the design to slow traffic and designate a place for pedestrians to cross the street.

All corners of blocks shall incorporate a paver design signifying the end of the block and the entrance to the street. The crosswalk shall continue the materials and theme across the street.

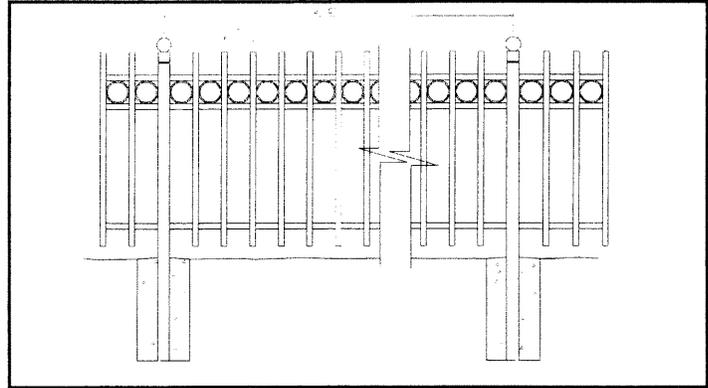


Figure 3.3.2
Ornamental Fence Detail

3.6.2 Sidewalk and Crosswalk Width.

Paver sidewalks should be 12'-0" minimum width from the face of the building to the back of the existing curb. Paver crosswalks shall be 8'-0" in width and have a 1'-0" wide band of concrete on either side. Total width of crosswalk shall be 10'-0".

Where possible, widen sidewalks to create

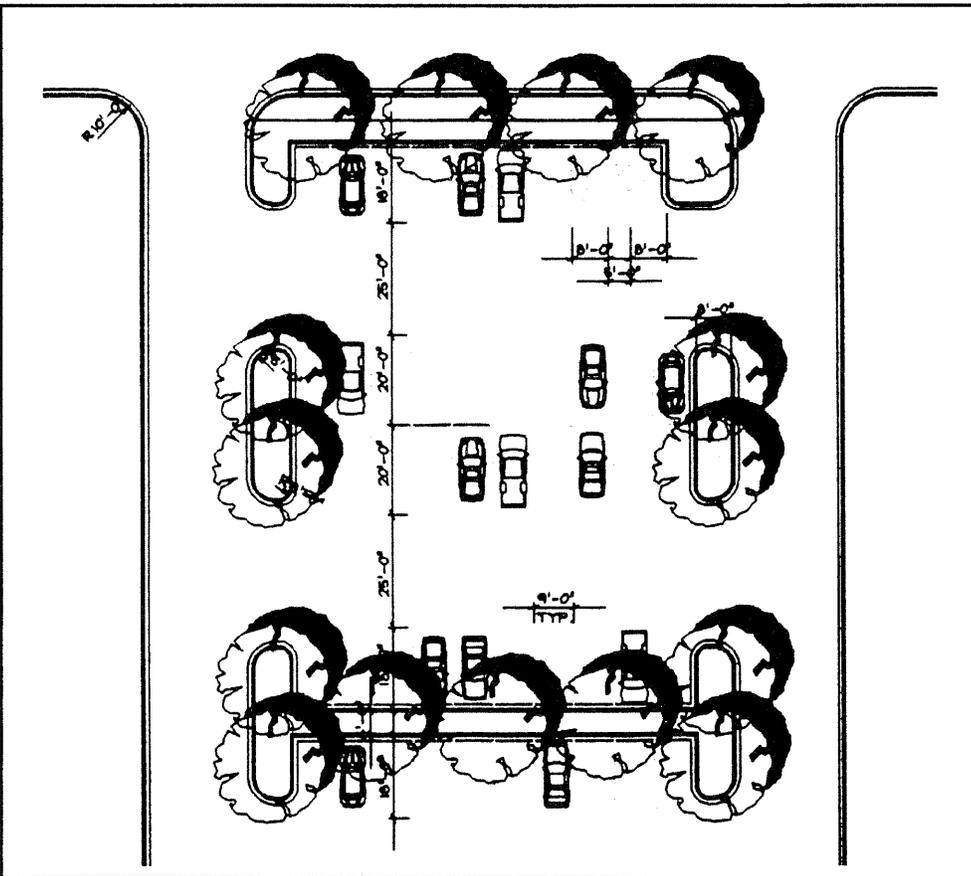
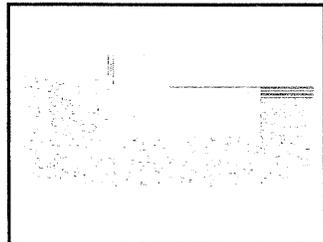


Figure 3.3.3a
Parking Lot Layout Plan

Figure 3.3.3b
B612 Curb Detail



small plazas for seating and fountains. These areas may be incorporated with an interesting building setback which will have to be approved by the City Planning Department. See example of a plaza area at 50th & France (Figure 3.6.2).

3.6.3 Pedestrian Cueing Areas

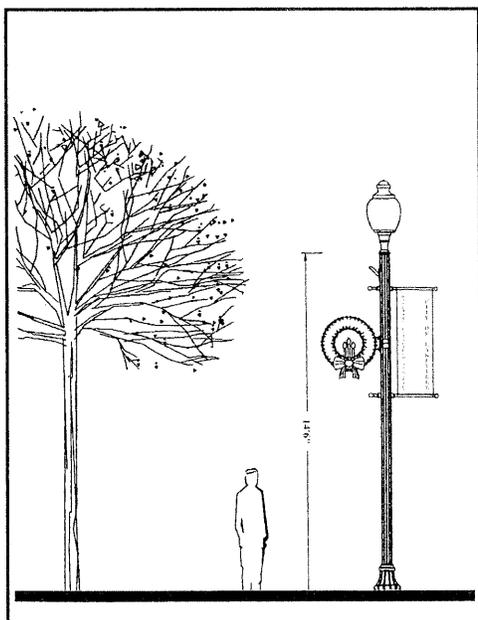
To encourage safety for pedestrian traffic across Main Street, there should be control to crosswalks with the addition of cueing areas. Cueing areas bring the curb and sidewalk out from behind the parking lanes to the drive lanes. This provides a better view for pedestrians to see the vehicular traffic. In addition to the cueing areas, a crosswalk made of pavers adds a definable area for pedestrians to cross Main Street. Both the cueing areas and crosswalk will also add a traffic calming affect to vehicles.

3.7 SITE SIGNAGE

3.7.1 Monument Signage.

Pylon signage should be prohibited within the Historic District. Use of monument signs (where applicable) may be allowed.

*Figure 3.4b
Lighting Detail*



*Figure 3.6.2
Plaza area at 50th & France*

Monument signs should not be greater than 5'-0" in height and 6'-0" in length. Monument signs should be constructed of the same exterior materials in which the new building is built. The same material ratios apply for the monument sign as does the building (see Chapter 4).

The inscription of monument signs shall not contain any graphic symbols, numerals or lettering other than those necessary to display the building or business name, kind or nature of business, business logo or year of establishment.

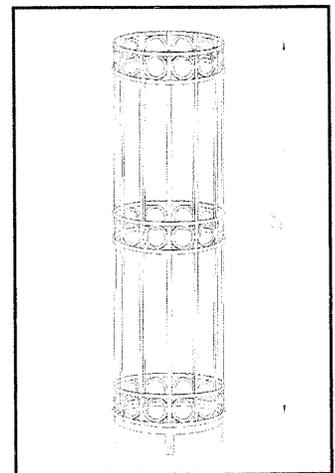
Monument signs may contain illuminated letters or concealed interior lighting. The letters shall be cut from any acceptable metal and may have a powder coated finish. The maximum height of the primary letters of the sign shall be 1'-0" and the maximum height of the secondary letters shall be 8". See Figure 3.7.1 (page 23), for typical monument signage.

Building signage should be encouraged vs. all other types of signage.

3.7.2 Directional signage.

Directional signage is imperative for ease of traffic circulation and enforcement. All signage in the Fairfield District should have a similar design and character as defined in the Corridor & Gateway Design Study.

*Figure 3.5.1
Tree Guard Detail*



A uniform steel structure should be incorporated into all street signage. This structure shall have a powder coated finish. See Figure 3.7.2a.

Additional consideration should be given to tenants within a building that is located on Main Street but where the tenants are not located facing directly on Main Street. Each tenant will be allowed a 30" x 8'-0" sign. An example of interior directional signage at locations of interior retail and office uses can be seen on the Figure 3.7.2b example.

3.7.3 Projecting Building Signs. Signs may project into the public right-of-way no more than 3'-0". See Figure 4.4.5 (page 29), for an example. Refer to section 4.4 for more guidelines on projecting building signs.

3.8 SCREENING

Exterior stored containers should not be allowed. Dumpster screening should be constructed of the same exterior materials in which the new or existing building is built. The same material ratios apply for the dumpster screening as do the building (see Chapter 4). Containers shall be fully

Figure 3.7.1
Typical monument signage

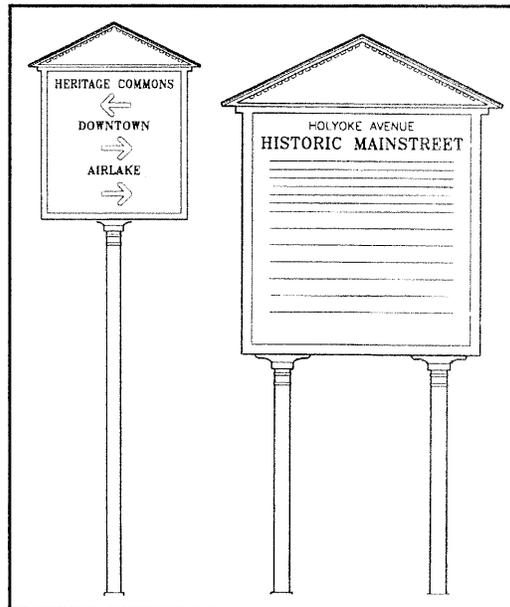
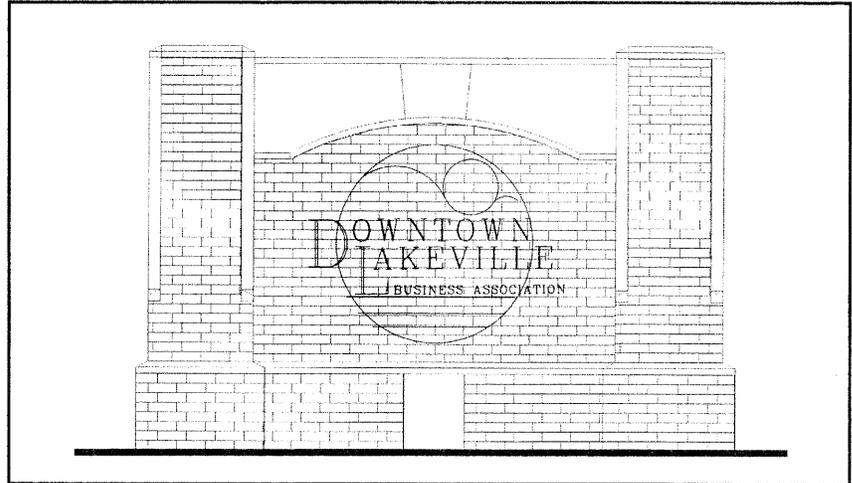


Figure 3.7.2a
Directional signage options as per details in the Corridor & Gateway Design Study by Barton-Aschman Associates, Inc.



Figure 3.7.2b
Interior directional signage

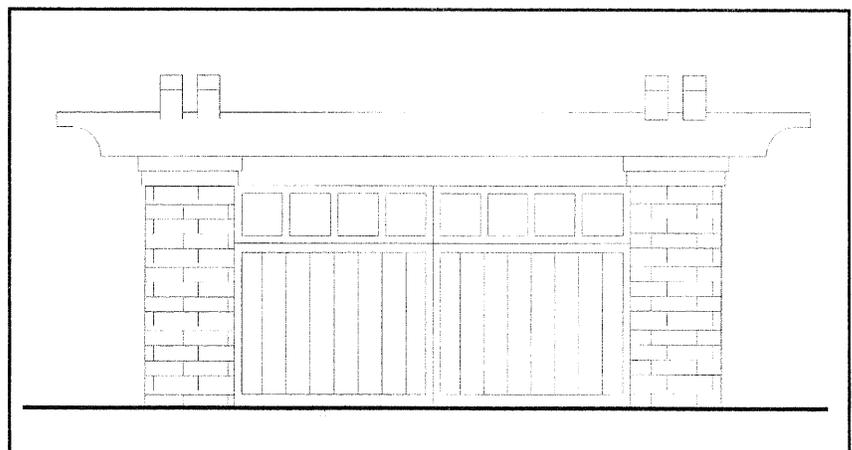


Figure 3.8
Typical dumpster screening enclosure.

enclosed and the screening mechanism shall be at least 90% opaque. The minimum height of the 90% opaque portion shall be 48" above finished grade. A trellis located above the 90% opaque portion is encouraged. Out-swinging gates for refuse removal may be 70% opaque. The minimum length and width of the enclosure shall be determined by the actual size of the dumpsters, but shall not exceed 15'-0" in length by 10'-0" in width (exterior dimensions). Landscaping shall soften the enclosure with mature shrubs of 5'-0" height. The dumpster enclosure is subject to review of the City Planning Department. See Figure 3.8 (page 23), for typical dumpster screening enclosure.

HISTORIC FAIRFIELD DISTRICT
OF DOWNTOWN LAKEVILLE

**DESIGN
GUIDELINES**

-SECTION FOUR-
COMMERCIAL BUILDING DEVELOPMENT
DESIGN GUIDELINES

**4.0 COMMERCIAL BUILDING
DEVELOPMENT DESIGN
GUIDELINES**

**4.1 MATERIALS AND
ACCEPTED RATIOS**

4.1.1 Materials.

Exterior facades of new buildings should be constructed of materials which will withstand the abuses of weathering and possible defacing due to vandalism. These materials should be easily maintained and attractive from any distance.

Acceptable materials for any new construction with street facing façades are rock face concrete masonry units, stone, sand molded modular sized brick, pre-cast concrete (as a trim material), stucco, and glass.

4.1.2 Ratios.

Exterior materials shall be limited in quantity by means of the following ratios:

- Rock Face Concrete Masonry Units
5% of the total area of the elevation in question
- Stucco/EIFS
15% of the total area of the elevation in question

Any combination of the following should total 75% of the elevation with the exception of the elevation facing Main Street which should be at least 30% glass:

- Stone (including limestone, marble, granite, etc.)
- Sand molded modular sized brick
- Architectural Pre-cast Concrete

4.1.3 Glazing.

The objective of supplying storefront glazing at the street level is not only to reflect a historical character but also to encourage window shopping. Street level glazing shall be limited to clear or glazing only. Spandrel glazing, mirrored finish glazing or glazing with greater than 10% tint is not acceptable at the street level.

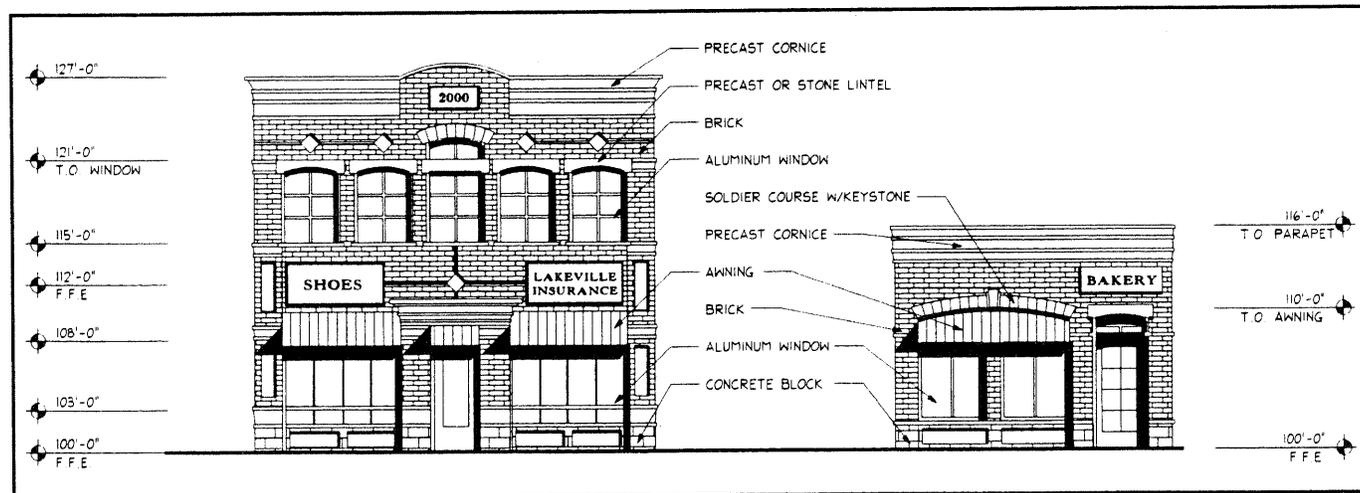
Second level glazing may be clear, tinted, frosted or spandrel glazing. However, spandrel glazing and back-lit frosted glazing shall be limited to 10% of the total area of glazing for the specific elevation. Back-lit frosted glazing is preferred over spandrel glazing.

4.2 BUILDING DESIGN

4.2.1 Objective.

The objective of creating design guidelines for individual buildings is to develop a unified character for all elements within the Fairfield District. The buildings and landscaping must convey the history of the dis-

*Figure 4.2.2
Typical One and Two Story Buildings*



trict while creating a sense of a true downtown. The intent of these guidelines is to allow for and encourage a variety of designs to create a unique overall character symbolic of a downtown that has been developed over a period of time.

4.2.2 Historical Context.

New buildings should be designed with historical ties to the existing significant buildings within the district and the design guidelines described in this study. Introduced character will create the architectural link to the historical past. The scale, massing, color, materials, texture and details of the new building should reflect the requirements set forth in Figure 4.2.2 (page 26).

4.2.3 Building Function.

New development should be organized with their axis perpendicular to Holyoke Avenue. Pedestrian Alleys shall link parking from behind stores to Main Street.

4.2.4 Appearance.

Each building should express their own individuality through size, shape of windows, details, cornice design, etc. This will enhance the sense of Downtown. New buildings should be designed to relate to the scale of pedestrians using a number of techniques: awnings to bring the scale down, heavier, more durable materials located at the base of the building; and lighter, more decorative materials located at the pinnacle of the building.

4.2.5 Height Restrictions

Corner buildings shall be a minimum of two stories in height and a maximum of four stories in height. The total height of the corner building shall not exceed 50'-0" above finished grade to the top of the parapet. Infill buildings between the corner buildings shall be one to two stories in height and shall not exceed 30'-0" in height to the top of the parapet. All commercial buildings located adjacent to Holy-

oke Avenue within the Historic District shall have a flat roof. Sloped roofs at tower elements are permitted. Tower elements are allowed to exceed the maximum height by 20% for one, two and three story buildings. Sloped roofs should be allowed on residential and religious buildings within the district.

4.2.6 Building Entrances.

The entrance to the building should be a significantly important part of the buildings overall design. The entrance should be easily identified from a distance and designed to be the highlight or pinnacle of the building. Many historically significant main street buildings were symmetrically designed with the entrance at the center of the building and the adjacent sides complementing, not competing, with the central entrance.

A canopy can add a distinguishing element and provide shelter for the occupants whether they are arriving or leaving the establishment. Double doors and additional lighting provide an inviting element to the entrance and allow vision into the space to demonstrate the hospitality found within.

4.2.7 Building Articulation.

During the construction boom of the early twentieth century, buildings were constructed quickly with durable materials. Owners of properties on the Main Street wanted their buildings to stand out from their adjacent neighbors with decorative brickwork, carved stone and colorful awnings. These buildings were constructed from the center of town to the outskirts. Since the property located at the center of town was much more desirable, the density in this area was greater than the buildings located on the fringes of the downtown. A number of elements are characteristic of a high density downtown.

Zero lot lines should be incorporated into

the planning of any future buildings located at the center of downtown. These setbacks will create a higher density at the center of town as is described on Figure 3.2, Lot Coverage Ratios. These zero lot line setbacks will also help the buildings to address the street by creating a pedestrian friendly space.

Each building should show its own individuality by means of shape, color, texture, etc. The buildings should appear as though they have been constructed at different times, by different contractors and have different owners. In addition, any building which spans an entire block and is constructed at one time, must be visually differentiated at strategic locations within the street facing facade. This will help to reduce the mass of the building and provide an interesting elevation at the street.

The buildings located on Main Street should have varying parapet heights. This will demonstrate that the buildings are different from each other and add interest to the streetscape. It will also allow for a possible entrance element.

The material selection should differentiate the buildings from each other. Varying the material palette from one building to the next will add to the authenticity of the downtown. Different materials, colors and textures all add to the visual interest.

The building should be broken into a number of bays. This will allow for the columns within the building to engage the streetscape and add a rhythm to the facade. The bays will also be a place to incorporate awnings, storefront window design and exterior building lighting.

Awnings have played a large part in introducing color to the downtown. Awnings should incorporate bright and interesting colors which enhance the color palette of the building facade materials. This element

will create a colorful and lively downtown.

As described elsewhere in this report, first floor windows should be aluminum storefront with transom windows where possible. This element will encourage window shopping and thus anchoring the individual shops to the streetscape.

However, second floor windows will serve a different function. This second floor space is reserved for more private functions; offices or apartments. The windows should respect these functions and the shapes should correspond. Tall, slen-

*Figure 4.2.7
Brickwork should incorporate extensive corbelling details, arches over windows and doors, and recessed sign bands.*



der, punched openings are appropriate for this area. Arched tops, columns framing the windows and decorative lintels are encouraged, lending to the scheme of a rugged and durable base metamorphosing the facade wall to a decorative parapet cornice.

The building should be rugged and durable at the base. Materials such as stone, rock face concrete block or pre-cast concrete should be incorporated. As it rises to the parapet, the structure should become more intricately detailed. Brickwork should incorporate extensive corbelling

details, arches over windows and doors, and recessed sign bands (see Figure 4.2.7). The top of the building should be terminated with a decorative cornice constructed of precast concrete, EIFS, stone, or decorative brickwork.

4.3 STORE FRONT DESIGN

4.3.1 Storefront Windows.

Storefront windows should be required for new buildings within the Fairfield District. Display windows, or windows allowing views into retail, office or lobby space, shall be 30% of the exterior wall area at the street level, or the first 15 feet above sidewalk grade. In no such case shall blank walls be longer than 20 feet, except where unavoidable due to specific building code requirements. All exterior sides of a bay window may be used in calculating the window percentages of this requirement.

4.3.2 Window Illumination.

Display windows should be illuminated until 11:00 PM. All lights should be properly baffled by architectural elements or be low brightness fixtures. Illumination levels should decrease to 10% of the total window illumination at a distance of three feet out from the window, measured at eye level height (5'-6").

4.3.3 Display Window Encroachments.

Display windows may be allowed to encroach up to two feet into the public right of way along the Main Street provided that a minimum of 8'-0" feet from window roadway curb is maintained. Display windows that encroach into the public right of way should not be continuous along the building façade. Windows should be limited to a length of 20 feet or less when fronting along a street and should provide a distance of 2 feet between windows.

4.4 PROJECTING BUILDING SIGNS

4.4.1 Location.

Projecting building signs should be located between 8 and 12 feet above sidewalk grade and should be limited to businesses which have storefronts or offices facing the street on which the sign is displayed.

4.4.2 Size.

For individual tenants or businesses the total surface square footage of projecting signs should not exceed 12 feet. Only one face of a flat or double-faced sign should be included in the computation of sign area.

Signs that identify a building or complex may exceed the size limitations contained in this section if it is determined by the City Planning Department that the visual impact of the signs are compatible with the signage of other buildings within the Fairfield District.

4.4.3 Projection.

Projecting signs may project from the building façade no more than 3'-0" into the public right-of-way.

4.4.4 Inscription.

The inscription of signs shall not contain any graphic symbols, numerals or lettering other than those necessary to display the business name, kind or nature of business, business logo or year of establishment.

4.4.5 Materials.

Projecting signs shall be constructed of ornamental metal, carved wood or cloth. Back-lit plastic sheet signs should not be permitted. The design of the supporting wires shall be incorporated into the overall sign design. Structure of any projecting signs shall be ornamentally designed. See Figure 4.4.5.

*Figure 4.4.5
Signs can project into the right-of-way.*



4.4.6 Illumination.

Projecting signs that are externally illuminated shall not emit more than 10% of the maximum candlepower to any point seen from the street or sidewalk at an angle less than 60 degrees from horizontal.

4.5 AWNINGS AND CANOPIES

Awnings and canopies shall be installed at all new and renovated buildings.

4.5.1 Projection.

Awnings and canopies shall not project more than 5 feet into the public right-of-way, except where located above an operable building or shop entrance, in which case the maximum projection shall not exceed 8 feet. In no event should the awning or canopy be supported by poles or other structural elements located in the public right-of-way. See Figure 4.5.1

4.5.2 Length.

Awnings and canopies should emphasize the rhythm of the façade bays, windows and entrances, and shall not continue uninterrupted along the building façade. Awnings and canopies shall not exceed 20 feet in length along the direction of the street, and shall be separated by a distance of at least 1'-4".

4.5.3 Height.

The bottom of awnings and canopies should be at least 8 feet above sidewalk grade, except in the case of a movable valance which may be 7 feet above sidewalk grade.

4.5.4 Illumination.

Back-lit awnings and canopies are not permitted.

4.5.5 Inscription.

Lettering on awnings and canopies should not exceed 16 inches in height, except that capital letters and the upward or downward extensions of lower case letters shall

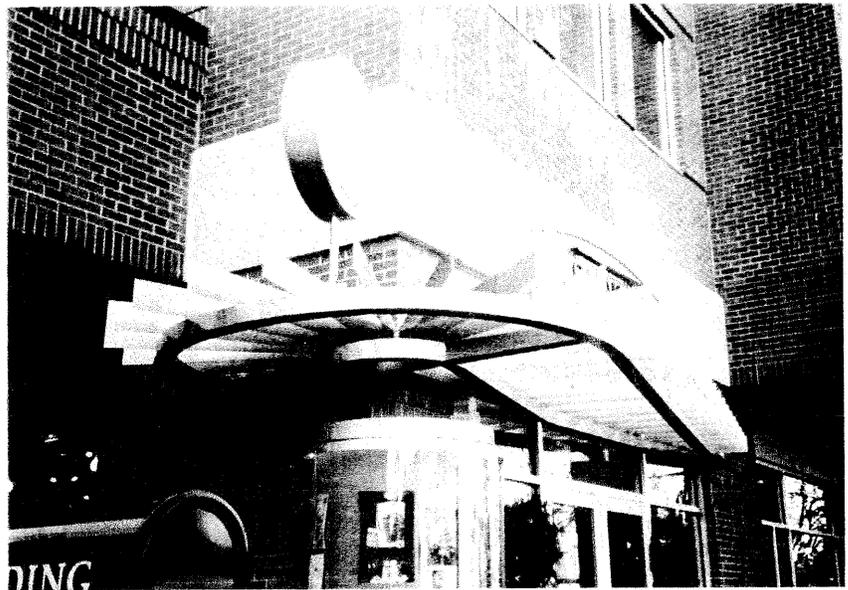
not exceed 36 inches in height. The inscription shall not contain any graphic symbols, numerals or lettering other than those necessary to display the business name, kind of business, business logo, year of establishment, or name of building.

4.5.6 Materials

Awning and canopy materials should be limited to cotton, acrylic or vinyl coated cotton, copper or bronze coated metal, or clear glass. Fabric covered awnings shall employ a striped pattern with historic references. Other materials may be used if approved by the City Planning Depart-

Figure 4.5.1

The entrance to Cooks of Crocus Hill at 50th & France has a metal canopy.



ment. Structural supports shall be constructed of steel and shall incorporate ornamental features.

4.6 BUILDING PERMANENT BANNERS

Colorful banners should be encouraged as a means of providing a retail character to the district.

4.6.1 Projection.

Banners should not project out from the building face more than 5 feet into the public right-of-way.

4.6.2 Size.

The total square footage of banners shall not exceed 25 feet.

4.6.3 Height.

The bottom of banners shall be at least 12 feet above sidewalk grade.

4.6.4 Spacing.

Banners should be spaced no closer than 20 feet apart.

4.6.5 Inscription.

The inscription of banners should not contain graphic symbols, numerals or lettering other than those necessary to display the business name, kind or nature of business, business logo or year of establishment. In addition, other permitted uses may include Fairfield District promotion or display, holiday or festival display, and flags.

4.7 SIDEWALK CAFES/RETAIL SALES**4.7.1 Location.**

Sidewalk cafes and retail sales are encouraged and should be located in the sidewalk area fronting the shop or café owner or operator's business.

Sidewalk cafes should be located either adjacent to the building or adjacent to the roadway depending upon the width of the sidewalk, pedestrian circulation patterns along the sidewalk, and the nature of the adjacent building facades including location of building and shop entrances and presence of display windows.

In either case, where the width of the sidewalk is 20 feet or less an 8 foot wide, clear and unencumbered path along the sidewalk shall be provided. It is the responsibility of the café owner or operator to keep their sidewalk clear at all times.

Sidewalk cafes adjacent to the roadway

should maintain a two foot setback from the back of the roadway curb.

Sidewalk cafes adjacent to the building should not obstruct any doorways necessary for safe and easy ingress and egress with adjoining buildings.

4.7.2 Furnishings.

Tables, chairs and other furnishings should be durable and sufficiently stable to prevent removal by winds. The appearance of furnishings shall also be high-quality and consistent with the image of the Fairfield District. Plastic furnishings will not be acceptable.

4.7.3 Removal.

Tables, chairs and other furnishings shall be promptly removed at the end of the day at the hour stipulated in the permit.

4.7.4 Cleaning.

All tables and chairs should be maintained in a clean condition at all times. Areas devoted to sidewalk cafes and retail sales shall be cleaned on a daily basis and shall be the responsibility of the café owner or operator. Cleaning shall include litter pick-up, trash removal and washing of the sidewalk to prevent stains. Sidewalk cafes not cleaned and maintained to the Fairfield District standards will be cleaned by the city maintenance crew with the costs assessed back to the café owner or operator.

At least one durable garbage receptacle within the sidewalk café zone shall be provided for sidewalk cafes without table service.

4.8 RENOVATED BUILDINGS

Significant buildings should be renovated to their original design to preserve their historic significance to the district. Details which have been lost due to weather or previous modifications should be replaced to their original condition or re-constructed using similar construction techniques and materials. Some buildings may have to be sandblasted, the masonry and limestone tuck-pointed, and the windows repaired or replaced as necessary.

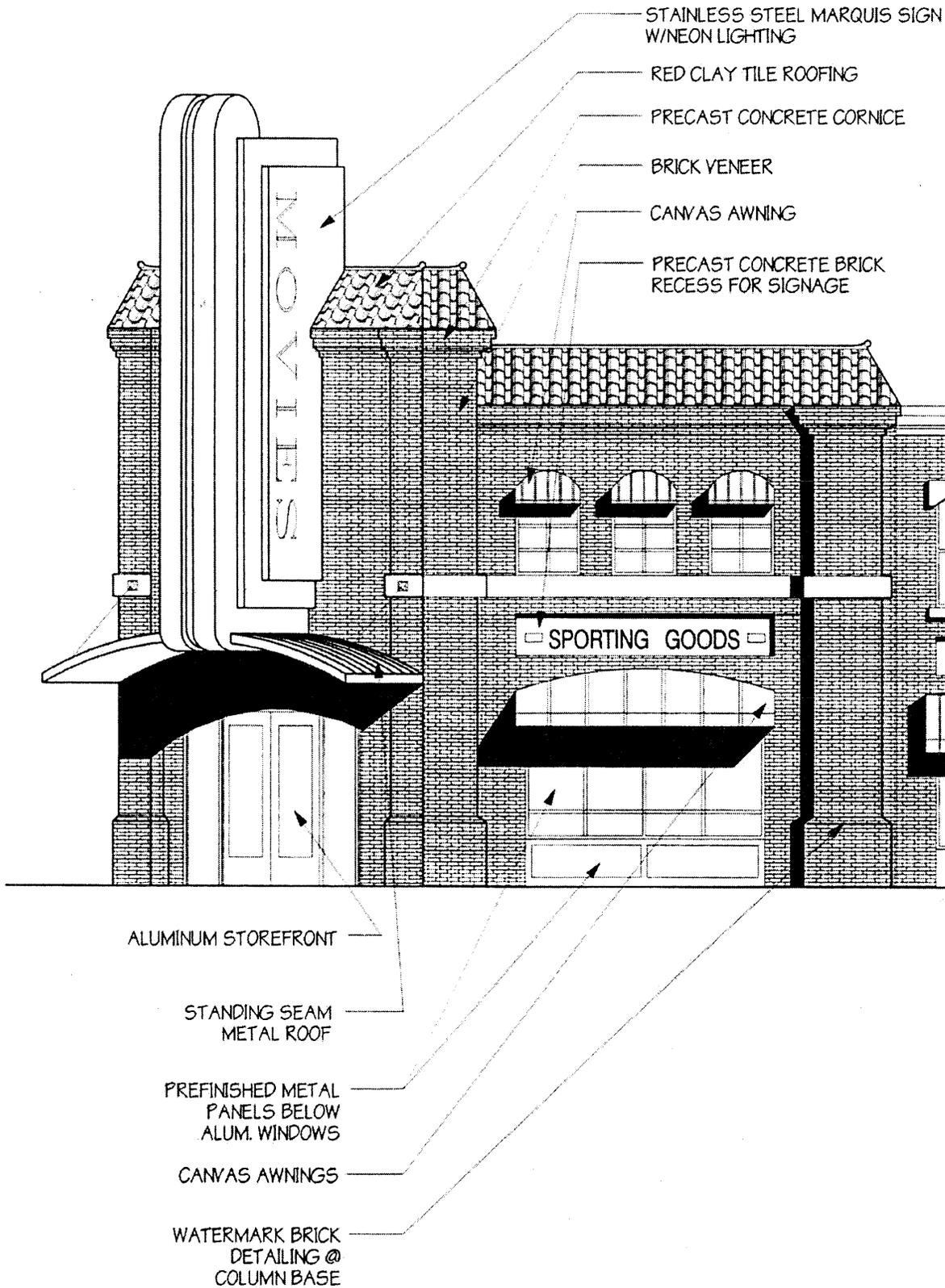
The following pages demonstrate the possible effects of renovation to existing historically significant buildings.

4.8.1 Daycare Center/School District Building

1. REMOVE SMALL ALUMINUM WINDOWS AND REPLACE WITH LARGE ALUMINUM STOREFRONT WINDOWS.
2. ADD BRICK VENEER ENGAGED COLUMNS TO BREAK UP FACADE.
3. ADD PRECAST LINTEL ARCHES WITH KEYSTONES TO ADD VISUAL INTEREST.
4. ADD AWNINGS TO PROVIDE COLOR TO STREETScape AND COMPLEMENT BUILDING COLORS.
5. PROVIDE SHINGLED MANSARD ROOF TO SCREEN ROOF TOP UNITS.
6. ADD CENTRAL ENTRANCE ELEMENT TO BREAK UP FACADE AND STEP ELEVATION UP TO TOWER AT CORNER.
7. PROVIDE BACKLIT FROSTED GLAZED WINDOWS AT FALSE SECOND FLOOR.
8. ADD JEFFERSONIAN TOWER ELEMENT AT CORNER WITH ARCHED ENTRANCE.



4.8.2a Retail/Lakeville Mall



4.8.2b Retail/Lakeville Mall

BRICK VENEER
DETAILING

PRECAST CONCRETE
DATE SIGN

RECESSED SPACE TO
PROVIDE DIVERSITY
TO ELEVATION

STONE COLUMN, BASE,
& CAPITOL

PRECAST CONCRETE
FINIAL DETAILING

PRECAST CONCRETE
CORNICE

PRECAST CONCRETE
LINTELS & SILLS



BRICK RECESS
FOR SIGNAGE

PREFINISHED METAL
PANELS BELOW
ALUM. WINDOWS

CANVAS AWNINGS

WATERMARK BRICK
DETAILING @
COLUMN BASE

4.8.2c Bank Building

KEY NOTES:

1. SAND-BLAST ENTIRE BUILDING FASCIADE TO EXPOSE EXISTING BRICK AND LIMESTONE SURFACES.
2. TUCKPOINT VENEER BRICK AND STONE PROBLEM AREAS.
3. ADD MULLIONS TO EXISTING WINDOWS AND FRAMES FOR HISTORIC AUTHENTICITY.



ADD PREFINISHED METAL
SIGNAGE BAND

ADD AWNING OVER ENTRANCE
& STOREFRONT TO PROVIDE
COLOR TO STOREFRONT

RESTORE LIMESTONE
DETAILING AT ENTRANCE

4.8.3a Dentist Building

REPLICATE ORIGINAL BRICK DETAIL @
PARAPET WITH EIFS

PRECAST CONCRETE BUILDING SIGN

REMOVE VERTICAL CEDAR SIDING AND
REPLACE W/ SPANDREL GLAZING

REMOVE VERTICAL CEDAR SIDING AND
REPLACE W/ PREFINISHED METAL PANELS
ABOVE NEW CURVED HANGING CANOPY
@ CORNER

ADD AWNINGS TO COMPLEMENT
BUILDING COLOR AND ENCOURAGE
WINDOW SHOPPING

ADD PREFINISHED METAL PANELS
TO BRICK VENEER BELOW ALUMINUM
WINDOWS



4.8.3b Retail Building



1. SANDBLAST ENTIRE BUILDING FACADE TO EXPOSE EXISTING BRICK AND LIMESTONE SURFACE
2. TUCKPOINT VENEER BRICK AND STONE @ PROBLEM AREAS
3. REMOVE CORRUGATED FIBERGLASS AND WOOD SIGN BAND. RESTORE ENTRY TO ORIGINAL CONDITION
4. REMOVE VERTICAL CEDAR SIDING.
5. REMOVE RESIDENTIAL GRADE DOORS AND WINDOWS AND REPLACE WITH COMMERCIAL GRADE ALUMINUM STOREFRONT
6. ADD MULLIONS TO EXISTING WINDOW FRAMES FOR HISTORIC AUTHENTICITY.
7. ADD DECORATIVE SIGNS @ BRICK RECESSED SIGN BANDS.
8. ADD AWNINGS TO PROVIDE COLOR TO STOREFRONT.
9. REMOVE WOOD CROSSBUCK BELOW WINDOWS AND REPLACE WITH PREFINISHED METAL PANELS INSTALLED OVER BRICK VENEER.

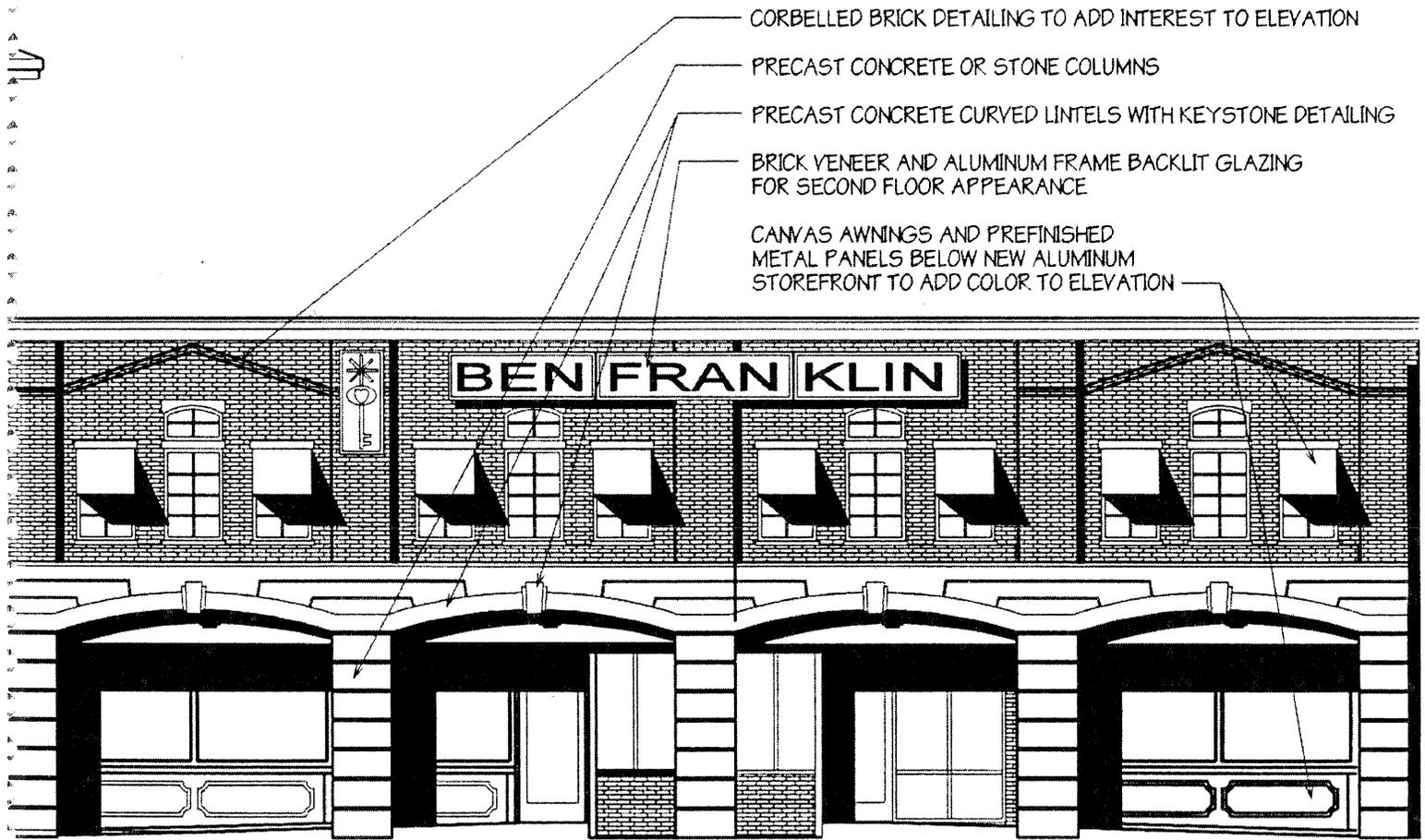
4.8.3c Fire Hall



1. SANDBLAST ENTIRE BUILDING FACADE TO EXPOSE EXISTING BRICK AND LIMESTONE SURFACE.
2. TUCKPOINT VENEER BRICK AND STONE @ PROBLEM AREAS
3. REMOVE RESIDENTIAL GRADE DOORS AND WINDOWS AND REPLACE WITH COMMERCIAL GRADE ALUMINUM STOREFRONT
4. REMOVE PAINTED T-111 PLYWOOD SIDING.
5. REMOVE AWNING AND REPLACE WITH SMALLER AWNING.
6. REPLICATE ORIGINAL FIRE HALL DOOR WITH ALUMINUM STOREFRONT.
7. INSTALL NEW ALUMINUM WINDOWS @ ALL TRANSOM LOCATIONS.
8. INSTALL BELLS IN BELL TOWER TO CHIME @ KEY HOURS OF THE DAY.

4.8.3d Ben Franklin Store

- REMOVE CEDAR SIDING MANSARD ROOF.
- REMOVE DIAGONAL SIDING @ COVERED WALKWAY.
- ADD MULLIONS TO EXISTING WINDOW FRAMES FOR HISTORIC AUTHENTICITY.
- REMOVE ENTIRE OVERHANG AND REPLACE WITH NEW STRUCTURE TO SUPPORT VENEER BRICK LOAD.

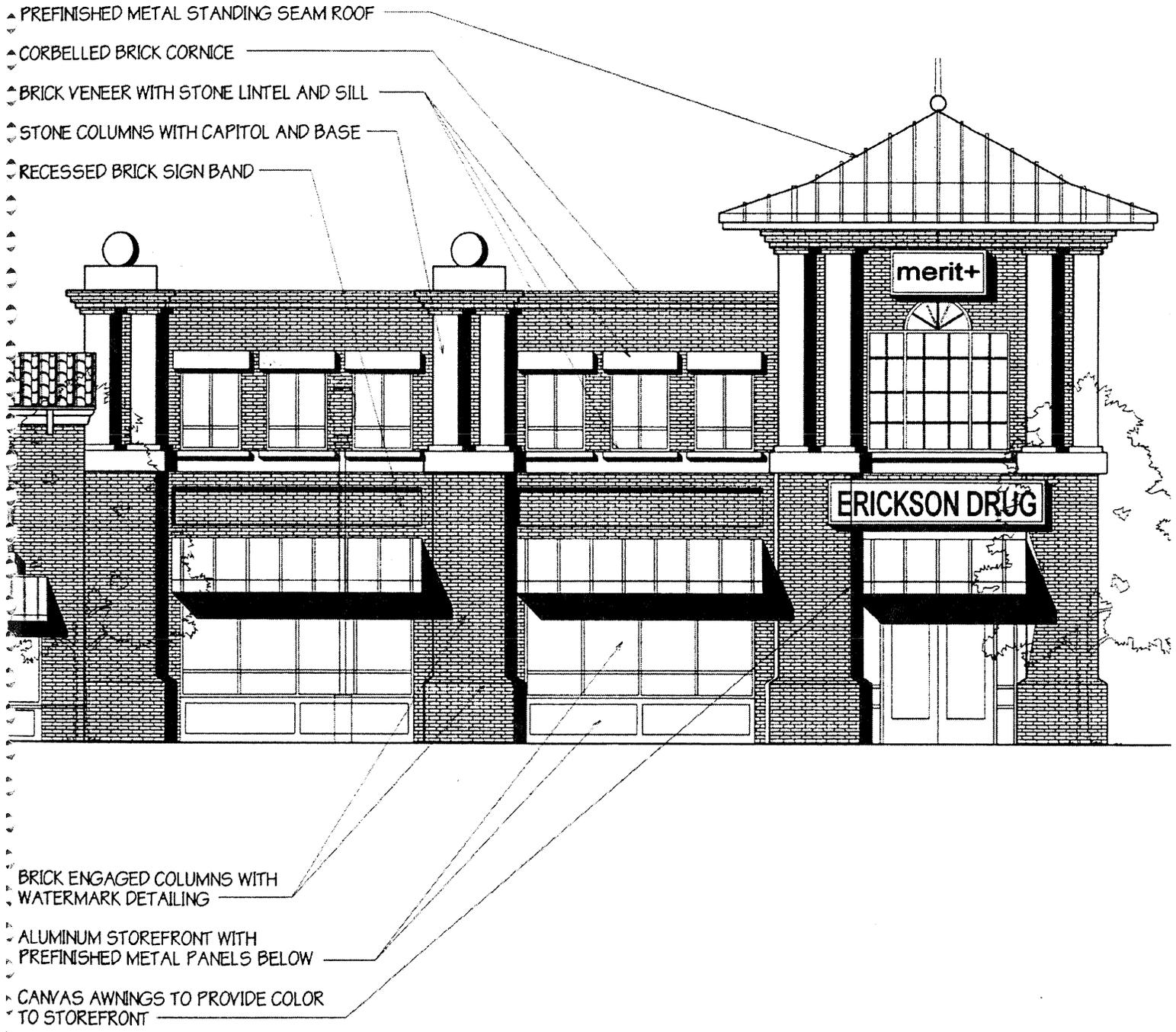


4.8.3e Trophy Shop

1. REMOVE PAINTED WOOD SIDING AND DECORATIVE WOOD PICKETS.
2. REMOVE RESIDENTIAL GRADE DOORS AND REPLACE WITH COMMERCIAL GRADE ALUMINUM STOREFRONT WITH TRANSOM GLAZING ABOVE.
3. REMOVE EXISTING STOREFRONT AND REPLACE WITH ENERGY EFFICIENT ALUMINUM STOREFRONT WITH ADDITIONAL MULLIONS.
4. INSTALL CLAY TILE ROOF WITH DECORATIVE BRACKETS.
5. ASSUMING BRICK IS HIDDEN BY EXISTING SIDING, SANDBLAST AS NECESSARY AND TUCKPOINT ANY PROBLEM AREAS @ BRICK VENEER AND STONE DETAILING.
6. INSTALL CANVAS AWNING TO PROVIDE COLOR TO STOREFRONT.
7. RECONSTRUCT BALCONY WITH STEEL RAILING AND DECORATIVE BRACKETS.
8. INSTALL PREFINISHED METAL PANEL BELOW WINDOW TO ACCENT AWNING.



4.8.3f Erickson Drug Store



4.9 PROPOSED BUILDINGS

The following pages demonstrate the possible architectural styles that buildings can look like.

4.9.1a New Retail Building



4.9.1b New Retail Building



4.9.1c New Retail Building



4.9.2 HRA Building

NOTES:

1. PROVIDE ENGAGED BRICK COLUMNS TO BREAK UP FACADE.
2. PROVIDE DECORATIVE AWNINGS BETWEEN BAYS TO ADD COLOR TO ELEVATIONS.
3. REPLACE WIDE WINDOWS WITH HIGHLY DETAILED NARROW WINDOWS.
4. BREAK UP LARGE EXPANSES OF BRICK WITH BRICK DETAIL AND ALTERNATIVE MATERIALS SUCH AS STUCCO/EIFS.
5. PROVIDE SLOPING PARAPET, DETAILED ENGAGED COLUMNS, AND PRECAST CONCRETE BUILDING SIGN AND AT COMMUNITY ROOM FOR VISUAL INTEREST.
6. ADD SMALL DORMERS AT CENTER PORTION OF BUILDING TO BREAK UP LONG ROOF LINE BETWEEN LARGE DORMERS.
7. LOWER ROOF 3' TO 4' TO DIMINISH SCALE OF BUILDING SO AS TO MATCH SCALE OF SURROUNDING BUILDINGS.
8. CREATE DETAILED TOWER AT CORNER USING ARCHITECTURAL ELEMENTS FROM THE BUILDING.
9. INSTALL PREFINISHED METAL PANELS BELOW STOREFRONT WINDOWS TO ADD COLOR AND DETAIL TO STREETSCAPE.



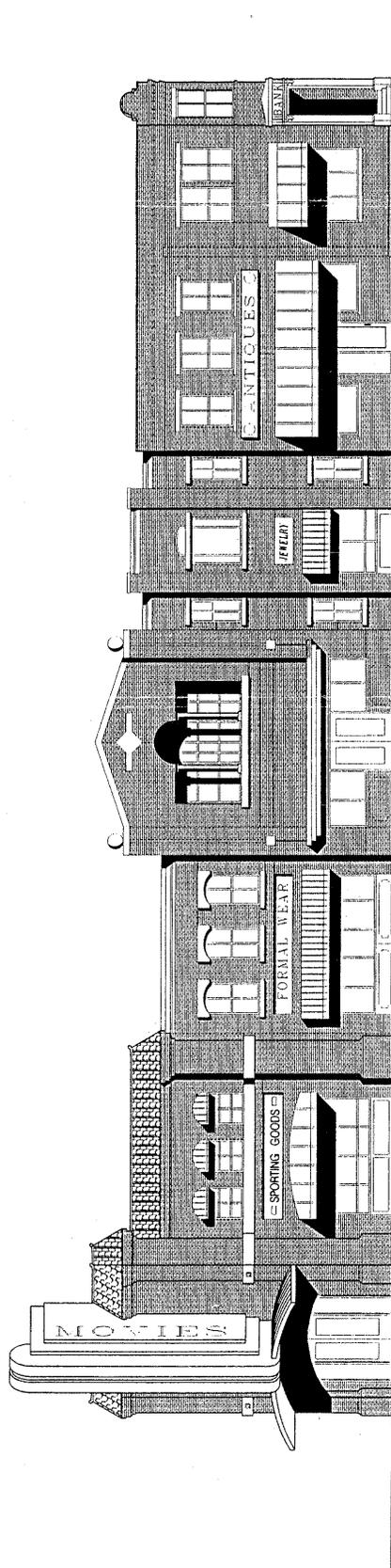


Figure 4.10a
Proposed Enggrens Mail Elevation

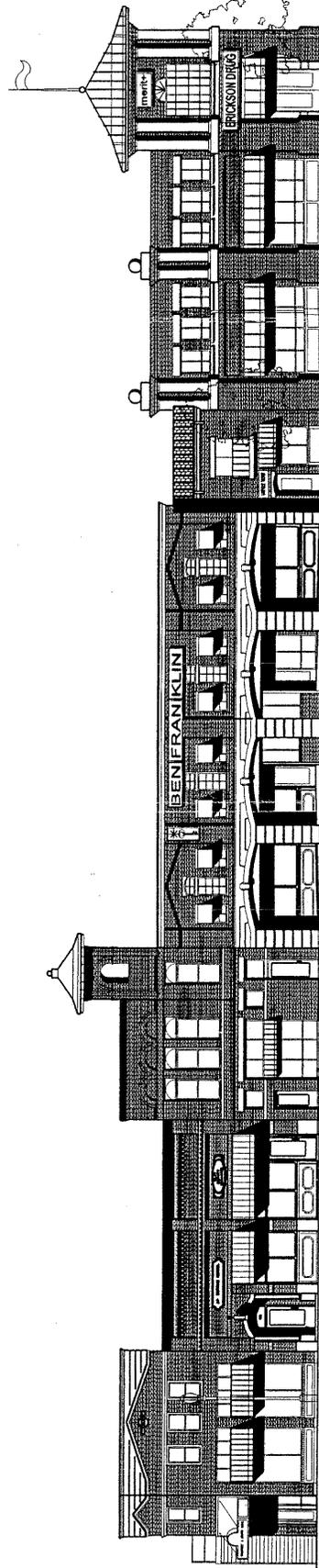


Figure 4.10b
Proposed Gregory J. Smith D.D.S. Office to Erickson Drug Elevation

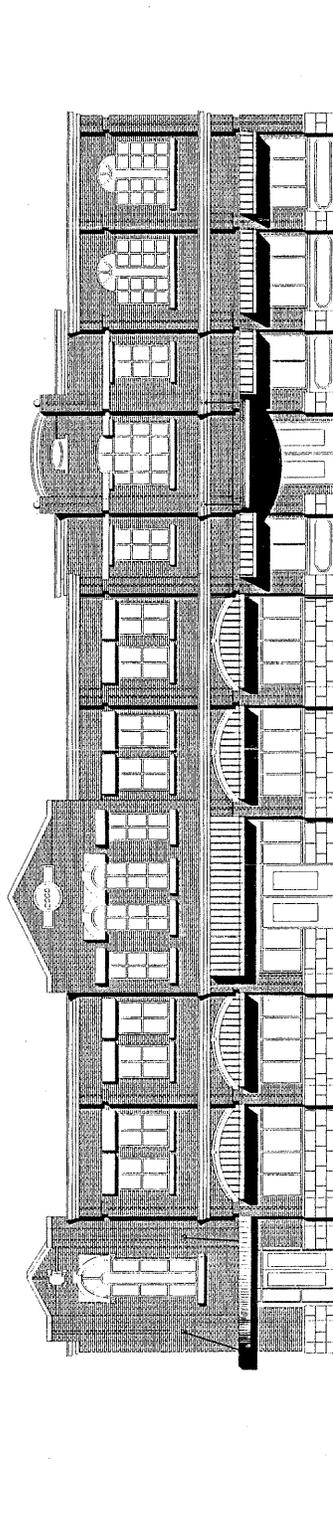


Figure 4.10c
Proposed New Retail Elevation



Figure 4.10d
Proposed HRA Building Elevation
With Historic Modifications

POSSIBLE SECOND STORY ADDITION

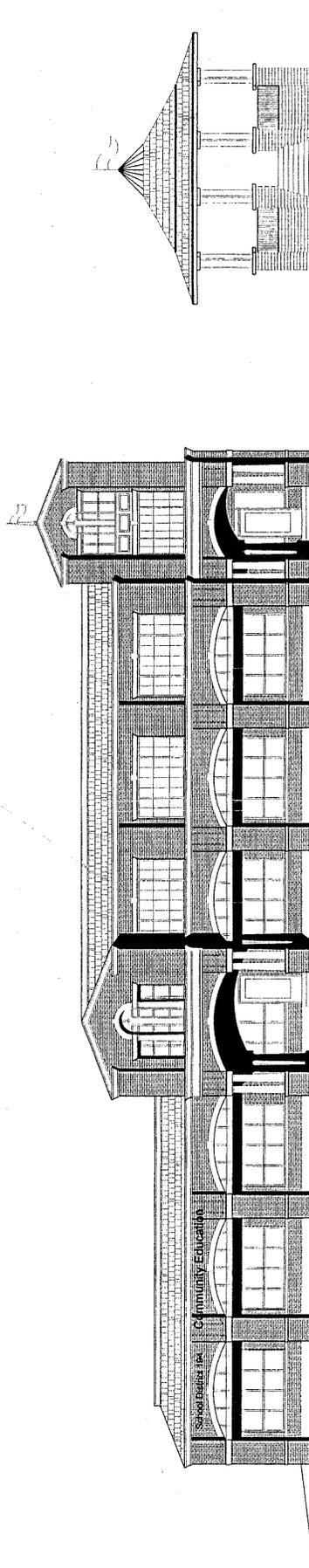
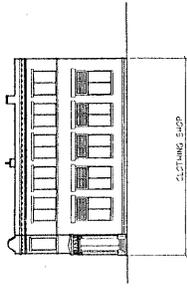
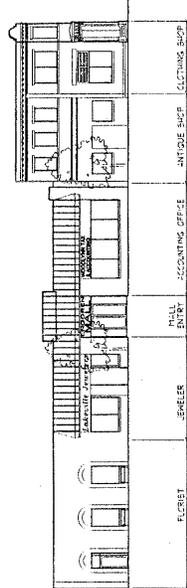


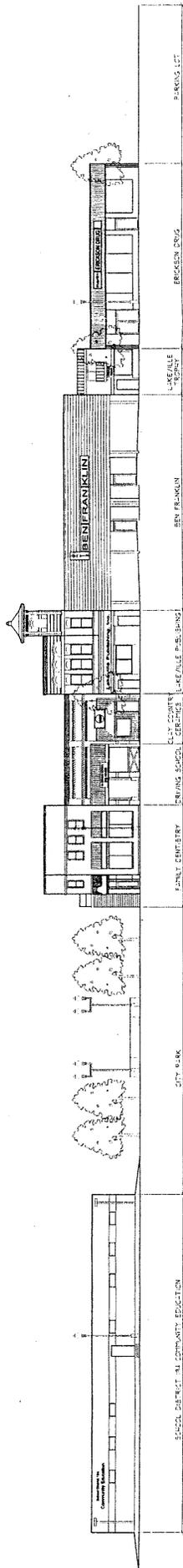
Figure 4.10e
Proposed Day Care Building Elevation



NORTH ELEVATION- FROM 208TH STREET



EAST ELEVATION- FROM HOLYOKE AVENUE



WEST ELEVATION- FROM HOLYOKE AVENUE

Figure 4.10f
Overall Elevations - Existing

HISTORIC FAIRFIELD DISTRICT
OF DOWNTOWN LAKEVILLE

**DESIGN
GUIDELINES**

-SECTION FIVE-
IMPLEMENTATION

5.0 IMPLEMENTATION

5.1 CITY GUIDELINE ADOPTION

Once the guidelines are approved by the Downtown Business Association they must be approved by the Lakeville City Council and adopted as a part of the city's zoning ordinances. The importance of this can only be realized in the future when these guidelines have been implemented.

However, it is anticipated that property values will be enhanced as the downtown grows and thrives in its own niche market while establishing a historical significance for the entire community for generations to come.

5.1 PRIVATE INVESTMENT

Even though this study is all encompassing and addresses many different aspects of potential development in the Downtown, it is intended to be a guideline that allows for flexibility in new development as well as existing building remodeling, renovations, and additions. It is important that existing property owners embrace the recommendations in this study, modify their properties and incorporate the recommendations in these guidelines and thereby set an example for future developments.

5.2 PUBLIC INVESTMENT

Just as important as the need for private pride investment is the public need to take pride in the downtown and invest in its future. The primary areas of public investment are as follows:

5.2.1 Town Square.

The Town Square, as previously discussed in this study, will become the center of the Downtown and as such the focus of planned events throughout the year. Modifications to Holyoke Avenue, the ex-

isting park, and the creation of the Train Depot Visitors Center and Farmers Market would require city involvement in order to complete. These could be paid for through the Parks and Recreation Department as a city owned and maintained park facility with the rent income generated from the Chamber of Commerce and Farmers Market going directly to the city. Possible sources of funding include park dedication fees, state and federal grants or a community bond referendum.

5.2.2 Tax Increment Financing District.

The existing downtown area currently has three tax increment districts. The funds from these districts are already committed to existing projects. The entire downtown is included in a tax increment project area which allows for the creation of site specific tax increment projects. Consideration should be given to expanding and creating a new tax increment district that would coincide with the existing downtown commercial district to assist and encourage new development. Tax increment funds can be used for land acquisition, demolition site and city improvement, soil correction or clean up of contaminated sites, to make these sites available for new development. These funds can also be used in public parking lots or structured parking ramps. All are enticements to encourage new development.

5.2.3 Community Development Block Grants.

The Lakeville City Council on January 3, 2000 approved the use of Community Development Block Grant (CDBG) funds to be used for storefront renovation in the historic downtown. The city must verify if these funds can in fact be used for this purpose and if so, what the restrictions are. It is the intent that funds used for this purpose would only be granted to property owners for historic renovations as in-

licated in these guidelines and not to be used for repairs that would otherwise be required as part of normal building maintenance.

5.2.4 Department of Trade and Economic Development Grant Fund.

When New Morning Windows office and manufacturing facility in the Fairfield Business Center was developed, they received a grant from the City of Lakeville. The city itself received a grant from the Minnesota Department of Trade and Economic Development.